



City of Burlington
Active Transportation and Safety Plan
September 15, 2025 – PUBLIC REVIEW DRAFT

Acknowledgements

City of Burlington Staff:

Brad Johnson Community Development Director

Miranda Sires, Planner

Brian Dempsey, City Engineer

Tyler Stamey, Director of Public Works

Burlington Planning Commission

Burlington City Council

Consultants

Consultants

Transpo Group, Inc.



Table of Contents

INTRODUCTION.....	1
CHAPTER 1 – GOALS AND POLICIES	2
CHAPTER 2 – EXISTING CONDITIONS	9
Previous Planning Efforts	9
Transportation Safety.....	10
Integration of Land Use and Transportation Planning	11
Who is Burlington Planning For?	13
Complete Streets	14
Level of Traffic Stress	15
CHAPTER 3 - ACTIVE TRANSPORTATION NETWORK.....	16
Evaluation of Citywide Pedestrian Network.....	17
Pedestrian Scoring Criteria and Prioritization.....	20
Recommended Pedestrian Improvements	21
Evaluation of Citywide Bicycle Network.....	27
Bicycle Scoring Criteria and Prioritization.....	30
Recommended Bicycle Improvements	31
CHAPTER 4. FUNDING AND IMPLEMENTATION STRATEGIES.....	37
Funding Mechanisms.....	37
Transportation Revenue Available for Improvements	38
Annual Tracking, Monitoring, and Reporting	52
CHAPTER 5 - INTERJURISDICTIONAL COORDINATION	53
Skagit County.....	53
City of Mount Vernon	53
Skagit Transit.....	57
Skagit Council of Governments	63
WSDOT.....	64
APPENDICES	66
APPENDIX A – SAFETY ANALYSIS.....	67
APPENDIX B – EQUITY ANALYSIS	87
APPENDIX C – VEHICLE LOS ANALYSIS.....	92

List of Figures

- Cover -3D Aerial Image of City of Burlington
- 2.1 Auto-Oriented vs. Multimodal Transportation Planning
- 2.2 Multifamily Developments Approved by City of Burlington, 2017-2025
- 2.3 Transportation User Scale of Vulnerability to Risk and Injury
- 2.4 Multimodal Transportation Requires a Careful Balance of Needs and Priorities
- 2.5 Bicycle LTS = Measure of User Comfort
- 2.6 Bicycle LTS = Measure of Facility Comfort
- 3.1 Active Transportation Network – Existing and Planned Pedestrian Facilities
- 3.2 2025 MMLOS Status of Pedestrian Active Transportation Network
- 3.3 Pedestrian Active Transportation Network Projects
- 3.4 Pedestrian Facility Illustrations and Descriptions
- 3.5 Pedestrian Crossing Type Illustrations and Descriptions
- 3.6 Active Transportation Network – Existing and Planned Bicycle Facilities
- 3.7 2025 MMLOS Status of Bicycle Active Transportation Network
- 3.8 Bicycle Active Transportation Network Projects
- 3.9 Bicycle Facility Type Illustrations, Descriptions, and Applications
- 4.1 Example of Road Diet to Install Bicycle Lanes, Burlington
- 4.2 – 4.9 Examples of Road Diet to Install Bicycle Lanes, Regional
- 5.1 Skagit County Minimum Active Transportation LOS Standard
- 5.2 Skagit County Active Transportation Network – West
- 5.3 U.S. Bike Route 10 (USBR 10) through San Juan County via WSDOT Ferry and Skagit County
- 5.4 Skagit Transit Route 101
- 5.5 Skagit Transit Route 208
- 5.6 Skagit Transit Route 301
- 5.7 Skagit Transit Route 513 Burlington to Anacortes
- 5.8 Skagit County At-Grade Railroad Crossings

Introduction

The City of Burlington, Washington is situated at the transportation crossroads of the Skagit region. While Interstate 5, State Route 20, the Burlington Northern Santa Fe Railroad, and the Skagit River have all provided critical transportation connections to surrounding consumer markets and regions for vehicles, goods, and services, they have also created significant mobility barriers for people trying to walk, bike, and roll in a wheelchair or mobility device along and across them.

Within Burlington, many streets currently have sidewalks, but there are also gaps in the sidewalk network for some parts of town. Dedicated on-street bicycle facilities exist on some arterials, but not on others. Several off-street multiuse trails are available for walking and biking, especially near the Skagit River, but some of these may not be ADA-compliant. As mentioned above, Interstate 5, SR 20, the BNSF railroad tracks, and the Skagit River make some areas within Burlington inaccessible to other areas.

Safety is always a critical issue for any city and Burlington strives to make streets safe, accessible, and comfortable for residents and visitors. The City completed a Local Road Safety Plan (LRSP) in 2020 and will complete an updated LRSP in 2026. Burlington is also participating in the completion of the Skagit Council of Governments (SCOG) Regional Safety Action Plan (RSAP), which is expected to be adopted by the end of 2025.

This Active Transportation and Safety (ATS) Plan supplements the Transportation Element of the Burlington Comprehensive Plan and:

- Establishes multimodal level of service (MMLOS) standards for intersections, pedestrian and bicycle facilities, and public transit facilities in the public right-of-way.
- Establishes a citywide Active Transportation Network (ATN) comprised of existing and planned sidewalks, bikeways, and multiuse trails.
- Evaluates the 2025 ATN MMLOS status and identifies ATN improvement projects
- Establishes scoring criteria used to evaluate ATN projects, and
- Recommends a prioritized list of ATN projects for the City to focus on implementing.

Balancing improvement needs with funding capacity is a critical strategy in planning, funding, and implementing a citywide multimodal transportation system. All mobility needs for all modes must be carefully considered, balanced, and implemented for the multimodal transportation system to provide space and safety for everyone. Due to funding constraints and limited financial resources, the City must also make investment decisions that involve trade-offs and compromises between numerous priorities competing for limited resources.

The ATS Plan includes an implementation strategy to help Burlington maximize multimodal transportation benefits while minimizing City costs. The ATS project list is designed to use local transportation funds to leverage state and federal grant funds, whenever possible, to help local funds go farther in completing the citywide ATN. Over time, this may require additional City staff to ensure that grants can be administered effectively, and projects delivered on time so that the City maintains an excellent reputation with grant funding agencies and continues to receive grants.

Chapter 1 – Goals and Policies

1.1. Goals and Policies

The following goals and policies are intended to ensure the City’s future multimodal transportation needs are met and to ensure a high level of coordination between the transportation planning and the other elements of the Comprehensive Plan, particularly the Land Use and Housing Elements. The bold headings below identify the City’s transportation goals. Each goal is followed by a list of policies. The goals describe *what* the City is trying to achieve, while the policies describe *how* the goals will be achieved.

Goal 1. Access and Resilience

Past development practices, particularly along Burlington Boulevard, have resulted in a disconnected and auto-dominated street network, created isolated parcels with limited access, and reduced the number of potential future routes. To facilitate the infill and redevelopment proposed in the land use and housing elements, the City should explore the feasibility of funding and constructing a finer grained and more robust multimodal street network. Over the next 20 years, the City will consider new multimodal corridors parallel to Burlington Boulevard, installation of ADA-compliant pedestrian crossings, opportunities for reducing average block lengths, and an additional east-west railroad crossing will be constructed, and average block lengths will be reduced. The transportation needs of the City’s residents and businesses will be met by providing additional flexibility and choices, without promoting any one mode to the detriment of another.

Policy

1. Improve the redundancy of the City’s arterial street network and provide quicker access to areas that are cutoff by physical barriers by applying the following strategies:
 - a. Conduct a study to determine the feasibility and cost of establishing one or more continuous north-south arterial street routes from Highway 20 to Whitmarsh Rd, in addition to Burlington Boulevard, compared to non-auto options to create multimodal connectivity throughout the City.
 - b. Pursue funding and construction for a new grade-separated crossing on the north-south BNSF right-of-way for the purpose of connecting South Spruce Street to South Anacortes Street.
 - c. Pursue opportunities to work with BNSF and other agencies to create additional grade separated crossings, crossing improvements, and quiet zones.
2. Establish and maintain a fully interconnected street system with a dense network of streets intersecting at regular, and frequent, intervals by considering the following strategies:
 - a. Dead end streets cul-de-sacs, looping roads, isolated development sites, and disconnected street patterns should be avoided and eliminated when possible.
 - b. New developments should be fully connected to the City’s street network and should be designed to facilitate the development of adjacent parcels and allow for the logical and orderly extension of streets, sidewalks, and paths with the overall objective of minimizing required travel distances.

3. Controlled intersections, with pedestrian crossings, should be provided at intervals of 600 feet, or less, along principal arterials.
4. Ensure that ADA-compliant sidewalk standards are enforced to keep sidewalks free from obstructions and that sidewalk widths are adequately sized based on land use context, traffic volumes and speeds, anticipated levels of use, and urban design considerations.
5. Work with rail operator and state and federal agencies to reduce wait times and blockages associated with train traffic and consider grade separation and crossing improvements.
6. Ensure adequate provisions are made for freight and deliveries:
 - a. Ensure new commercial and industrial developments incorporate design features to accommodate freight handling and deliveries and reduce conflicts between general traffic and trucks.
 - b. Preserve or expand the current supply of industrially zoned land along rail lines to accommodate existing and future developments that rely on, or benefit from, rail access.
 - c. Design standards and code requirements for land designated CI on the Comprehensive Plan Map should reflect the needs of freight access, commercial vehicles traffic, and industrial development.
7. Consider the need of those who are dependent on cars for transportation, including families with young children, people with disabilities, and those who have difficulty getting around by providing conveniently located parking spaces at parks and public facilities, and by providing ADA accessible curb parking at regular intervals in the City's downtown.

Goal 2. Internal Consistency

The City will ensure that transportation plans and investments are consistent with the land use and housing elements of the Comprehensive Plan with a focus on infill and mixed-use development to support active transportation and transit, as well as reductions of vehicle miles traveled and greenhouse gas emissions, while accommodating community growth.

Policy

1. The Capital Improvement Plan (CIP) and Transportation Improvement Plan (TIP) shall be consistent with the Comprehensive Plan and shall be reviewed annually by the Planning Commission for internal consistency and consistency with the Comprehensive Plan.
2. The City's Comprehensive Plan, CIP, and TIP, and the Skagit Regional Transportation Plan shall be consistent with one another.
3. Consistent with RCW 36.70A.070.(6)(c) Skagit Transit (SKAT) should work with the Skagit Council of Governments (SCOG), which is the Regional Transportation Planning Organization (RTPO) to ensure its Six-Year Transit Development Plans, capital expenditures, and service levels are consistent with the comprehensive plans adopted by the cities in Skagit County. SKAT should work with other agencies and jurisdictions in Skagit

County to develop regionally coordinated level of service (LOS) standards that support urban development in established urban growth areas.

4. Develop design specifications and cross section detail for different street classifications and segments. All subsequent public and private street improvements shall be consistent with the adopted standards and specifications.
5. Consistent with RCW 36.70A.103 and RCW 36.70A.070.(6)(c) State Agencies, including WSDOT, shall ensure their plans, capital expenditures, and actions are consistent with the Burlington Comprehensive Plan and the Skagit County Countywide Planning Policies (CPPs).
6. WSDOT shall fully comply with all applicable Washington State laws by adopting and implementing Multimodal Level of Service (MMLoS) standards for highways of statewide significance and by ensuring State transportation plans, investments, and improvements are consistent with Washington State laws requiring reductions in per-capita vehicle miles traveled (VMT) and transportation related greenhouse gas (GHG) emissions (RCW 47.01.440 and 70A.45.020).

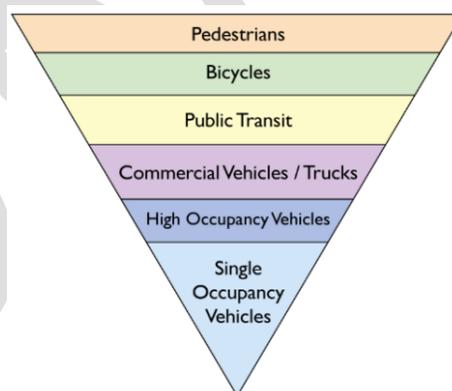
Goal 3. Transportation Demand Management

New road capacity and infrastructure are extremely expensive. Future transportation expenditures will be minimized by reducing travel distances, making it possible to accomplish multiple tasks in one trip, and by shifting demand to less costly and more efficient modes. By 2045 the number of vehicle miles traveled and greenhouse gas emissions, on a per capita basis, will be reduced and a higher percentage of local workers will commute by walking, bicycling, riding transit, or work from home.

Policy

1. Improve convenience and reduce trip lengths by permitting and encouraging intensive mixed-use, residential, and commercial uses in the Commercial Core, Northern Gateway, and Downtown growth areas. Special consideration should be given to authorizing the construction of infill housing, such as townhomes, duplexes, and small lot development within walking distance of commercial areas.
2. Encourage more trips to be made by walking, biking, rolling, and riding transit by taking the following actions:
 - a. Provide and maintain sidewalks of adequate width along both sides of streets and install ADA-compliant curb ramps and pedestrian crossings at regular intervals.
 - b. Ensure that new development includes direct, convenient, and ADA-compliant pedestrian connections.
 - c. Enhance pedestrian amenities, such as wider sidewalks, landscaping, and additional lighting, should be provided in the City's most important commercial corridors where high volumes of pedestrian traffic are expected or encouraged.

- d. Develop and implement an active transportation wayfinding and signage program that identifies the location of multi-use paths, bicycle lanes, public facilities, parks, schools, and transit routes.
 - e. Provide bicycle racks or storage facilities at public facilities such as schools, municipal offices, and parks, Bicycle racks and storage facilities shall also be required for large commercial and residential developments.
3. Work with Skagit Transit to improve transit service along the entire Burlington Boulevard corridor, encourage and support minimum levels of transit service based on transit bus headways, and encourage Skagit Transit to collect passenger counts in each jurisdiction served and by route segments. Transit service along high performing urban routes should be prioritized over low performing rural routes.
 4. Identify active transportation improvements and work with WSDOT to provide for the safe movement of pedestrians and bicycles along the SR-20 corridor, consistent with [RCW 47.04.035 Complete Streets Principals](#) and the [2020 WSDOT Active Transportation Plan](#).
 5. Emphasize the safety needs of vulnerable road users in planning and funding transportation improvement projects by promoting transportation mode priorities as follows:
 - a. Pedestrian
 - b. Bicycle
 - c. Transit
 - d. Freight
 - e. Carpool
 - f. Single Occupant Vehicle



6. Pedestrian and Bicycle LOS Standards are based on degree of completeness of sidewalk and bikeway connections as measured on the citywide Active Transportation Network. The LOS standards shown in green, orange, and red emphasize system completion of sidewalks, bikeways, or multi-use trails on arterial and collector roadways.

Active Transportation Levels of Service (LOS) Standards

- A **GREEN LOS** indicates that an active transportation network segment meets minimum

roadway standards and has active mode facilities on both sides of the street, or a paved two-way multiuse pathway on one side of the street.

- An **ORANGE LOS** indicates that an active transportation network segment partially meets minimum roadway standards and/or has facilities only on one side of the roadway, when both sides would be preferred.
- A **RED LOS** indicates that an active transportation network segment does not have active mode facilities provided and is considered inadequate.

LOS	Active Transportation Standard
	Meets minimum MMLOS standards; has facilities on both sides of the road or two-way multiuse pathway on one side
	Partially meets minimum MMLOS standards; facilities on only one side of the roadway; both sides would be preferred
	No designated facilities are provided for active transportation users and is considered inadequate

7. The Transit LOS Standard is based on ADA accessibility of Skagit Transit bus stops within the public road right-of-way. The prioritization and completion of ADA upgrades at all bus stops provides mutual benefit to the City of Burlington and Skagit Transit.

LOS	Transit Standard
	ADA Compliant Pedestrian Connection to Transit Stop
	Non-compliant Pedestrian Connection to Transit Stop
	Missing Pedestrian Connection to Transit Stop

Transit Level of Service Standards

8. Complete an Americans with Disabilities Act (ADA) Transition Plan including a financial plan for constructing and replacing ADA compliant ramps and sidewalks. Develop a prioritized list of ADA compliant routes throughout town, which provide access to key city amenities and services for people with disabilities and implement facilities improvements based on these priorities.
9. Periodically update the Complete Streets ordinance to ensure that City policies and practices are consistent with current best practices in transportation planning.

Goal 4. Health and Safety

Many people are injured or killed in transportation related crashes each year and, increasingly, the most vulnerable road users are at the highest risk. Research shows that a lack of physical activity is often a significant contributor to heart disease, obesity, diabetes, and other poor health conditions. Active transportation and safety improvements to the transportation system can reduce the probability of serious accidents and improve public health. By 2045 the number of people who engage in physically active transportation, such as walking or bicycling, will be increased, a larger percentage of the City’s population will live within walking distance of a school or park, and the number of deaths and serious injury crashes will be reduced.

Policy

1. Establish a low-stress network of ADA-compliant sidewalks, multi-use paths, and bicycle lanes that connect to parks, schools and public attractions.
2. Plan and provide enhanced pedestrian and transit amenities along Burlington streets served by Skagit Transit routes, such as ADA-compliant curb ramps, wider sidewalks, covered transit shelters, pedestrian oriented signage and lighting, landscaped buffers, user-activated, high-visibility crosswalks improvements coordinated with Skagit Transit bus stops, and curb extensions to increase visibility of people wanting to cross.
3. Work with the Burlington-Edison School District to establish and implement a wayfinding and signage system that identifies safe walking, rolling, and cycling routes to school and work. Provide this information to students and employees to promote fewer vehicle trips, as well as public health benefits.
4. Reduce speeding and unsafe driving through law enforcement and traffic calming measures such as curb extensions to tighten corner turning radius, narrower vehicle travel lanes, installation or reorganization of on-street parking to narrow driving space, install neighborhood intersection traffic islands, and, where feasible, the funding and construction of a dense and fully connected street-network.
5. Continually monitor and evaluate emergency response times and incorporate accident information provided by the City's police and fire departments into future transportation plans.
6. Enhance the safety of the SR-20 corridor including, but not limited to the construction of, controlled intersections, pedestrian crossings, sidewalks, traffic calming measures, streetscape improvements, and multiuse paths.

Goal 5. Financial Stability

As Burlington grows, competition for scarce physical space and financial resources will increase and trade-offs must be considered to provide a safe, equitable, and balanced multimodal transportation system. While new higher-density development can have traffic impacts, it can also reduce trip lengths, provide more opportunities for residents to walk, bike, and ride transit, and is required to fund and construct sidewalks and bikeways. The City will strive to balance the costs and benefits of active transportation and transit infrastructure between both public and private funding sources.

Policy

1. When evaluating major land use changes, such as urban growth area expansions and annexations, the short- and long-term financial implications of transportation needs shall be considered. Preference and priority shall be given to higher-density, mixed-use, and transit-supportive land use options that reduce the need for expensive vehicle-oriented transportation infrastructure by reducing trip lengths or shifting demand to less expensive modes, such as walking, biking, rolling, and transit.

- 2.** Transportation expenditure activities should generally be prioritized consistent with transportation safety and modal priorities listed in Goal 3, Policy 5 and generally in the following order:
 - a.** Maintenance and repair, coupled with identified multimodal improvements, whenever feasible.
 - b.** Safety improvements, including the provision of sidewalks, pedestrian crossings, streetscape improvements, and neighborhood traffic calming measures.
 - c.** New street or active transportation connections within the City's existing municipal boundaries that improve multimodal access to underserved areas.
 - d.** Multimodal improvements necessitated by the annexation of land in the City's urban growth area.
 - e.** Multimodal improvements necessitated by an expansion of the City's urban growth area.
- 3.** Promote and support the Transportation Benefit District (TBD) as a stable, sustainable, and dedicated local revenue source for multimodal transportation improvements. Expenditure of TBD revenue shall be prioritized and consistent with the goals and policies of this plan.
- 4.** Transportation Impact Fee incentives should be established to reflect the disproportionate transportation impacts of low-density development in outlying areas and to recognize the multimodal benefits of directing higher density growth to central areas, such as the Commercial Core, Northern Gateway, and Downtown areas.
- 5.** Consider partnering with Skagit County to study the potential benefits and challenges of establishing a program to collect transportation impact fees in unincorporated Skagit County for the purposes of mitigating the impacts of unincorporated development on City transportation infrastructure.

Chapter 2 – Existing Conditions

Consistent with Chapter 1 Goals and Policies, Chapter 2 provides information on how and why this Active Transportation and Safety (ATS) Plan is building on previous planning work with a new focus on walking and biking facilities for vulnerable road users. Chapter 3 documents the evaluation of existing conditions and recommendations for new walking and biking facility connections across the city to create a citywide Active Transportation Network (ATN). The recommended improvements identified in this section are sized for feasibility of construction based on cost and complexity and include “planning-level” cost estimates. A planning level cost estimate is a rough estimate used for preparing capital plans and evaluating future budget needs. Chapter 4 provides a high-level forecast of City revenue available for transportation improvements and strategies for successful implementation. Chapter 5 documents intergovernmental coordination and relevant previous or on-going studies and plans conducted by other agencies that have an associated interest in this plan.

Previous Planning Efforts

Currently, Burlington has an extensive citywide pedestrian network and a variety of multiuse pathway facilities, but very few on-street dedicated bicycle facilities. The 2016 Transportation Element proposed some pedestrian enhancements, primarily focused along and around the Burlington Boulevard corridor, and several planned bicycle facilities, but lacked in-depth evaluation of street conditions, land use context, or recommendations for bicycle facility types. Historically, Burlington’s citywide transportation system has predominantly been auto oriented, but the City has been making significant investments in active transportation infrastructure to serve non-auto mobility needs and intends to continue to transform streets from auto-oriented to multimodal, as illustrated in Figure 2.1. The foundation for this change begins with the GMA requirement to adopt multimodal level of service (MMLOS) standards that are appropriate for the City of Burlington. The term multimodal is inclusive of pedestrian, bicycle, transit, and vehicle modes of transportation. Level of Service (LOS) standards are quantitative and qualitative measures used to determine if transportation facilities are available and functioning as intended by the City.

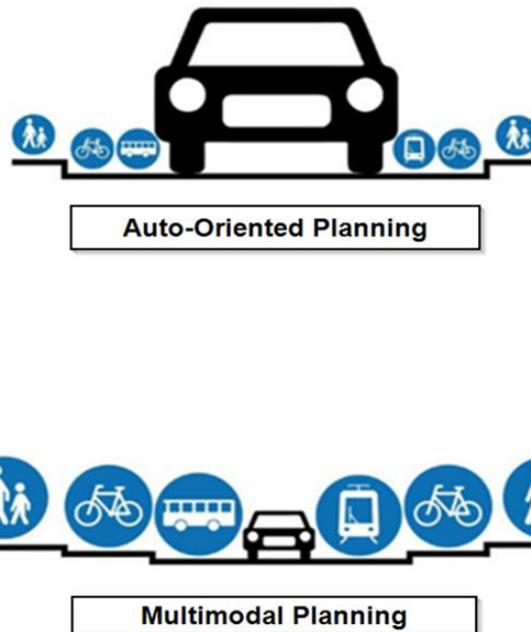


Figure 2.1. Evolution from Auto-Oriented to Multimodal Transportation Planning

Transportation Safety

Burlington places a high priority on safety for all users, and this Active Transportation and Safety Plan focuses on safety for the most vulnerable road users on the citywide multimodal transportation system. A safety analysis was performed for all streets and state highways within Burlington City limits using WSDOT crash data for years 2020-2024 with findings presented in detail in Appendix A.

Safety is used as one of the primary criteria in the evaluation of existing and planned active transportation facilities. Documented crash locations involving vehicles and pedestrians or bicyclists receive higher scores and if these crashes resulted in serious injuries or fatalities, then scores are higher. Fortunately, compared to other cities, Burlington has not had a large number of fatal or serious injury crashes involving pedestrians and bicyclists, but every one of these requires the City's attention.

Where active transportation safety issues have been documented and scored, recommendations are made for sidewalk, crossing, or bikeway improvements based on transportation industry-accepted methods, such as [FHWA's Proven Safety Countermeasures](#) or the [Crash Modification Factors Clearinghouse](#), both of which document the effectiveness of safety strategies in reducing roadway fatalities and serious injuries on public streets. These safety strategies are designed for all road users and all kinds of roads -from rural to urban, from high-volume freeways to less traveled two-lane State and county roads, from signalized crossings to horizontal curves, and everything in between. Each countermeasure addresses at least one safety focus area – speed management,

intersections, roadway departures, or pedestrians/bicyclists – while others, such as [Local Road Safety Plans](#), are crosscutting strategies that address multiple safety focus areas. One of the implementation strategies for this Active Transportation and Safety (ATS) Plan is to develop a 2026 Local Road Safety Plan (LRSP), which will allow the City to apply for federal Highway Safety Improvement Program (HSIP) funds, administered by WSDOT through the [City Safety Program](#). While this ATS Plan focuses on the needs of people walking and biking, the 2026 LRSP will broaden the focus to all road users and both spot and systemic transportation safety improvements. The City also has the option to update the LRSP every two years and apply for HSIP grant funds and the LRSP will help to support grant applications for several other grant programs.

Integration of Land Use and Transportation Planning

Changes to the local land use environment are likely to have the largest single impact on active and multimodal transportation needs and benefits. Mixed use infill development allows more people to live close to where they work, shop, play, and entertain themselves. If well-connected sidewalks, bikeways, and transit service are available, then residents can choose to walk, bike, roll, and ride transit to their destinations instead of driving vehicles for most trips. This provides numerous benefits, including housing more people on less land, reduced vehicle miles traveled, reduced greenhouse gas emissions, increase in walking, biking, and transit trips, an overall increase in public health, increased quality of life for residents, and a more attractive place for visitors to spend time and money.

The City of Burlington has made great strides in land use planning in the past 10 years, has invested in active and multimodal transportation infrastructure, and is working with Skagit Transit, Skagit County, the Skagit Council of Governments (SCOG), and WSDOT on transportation safety and regional mobility plans. Several multi-story multifamily residential developments have been constructed along or near major transportation corridors that are served by sidewalks and Skagit Transit bus routes. As shown in Figure 2.2 below, most new multifamily developments about Skagit Transit bus routes and all multifamily development are within a ¼-mile (5-minute walk, bike, or roll) to Skagit Transit bus stops.

This type of transit-supportive development is a tremendous benefit for transit-dependent bus riders, low-income residents, car-free households, and transit-by-choice riders because they can safely and conveniently walk, bike, or roll to bus stops instead of having to rely on a vehicle. Continuing and magnifying this intentional and integrated land use-transportation planning approach will be a key to success for Burlington to accommodate future growth and to develop a citywide multimodal transportation system. The scoring criteria used to prioritize sidewalks and bikeways in Chapter 3 and the implementation strategies in Chapter 4 reflect this approach.

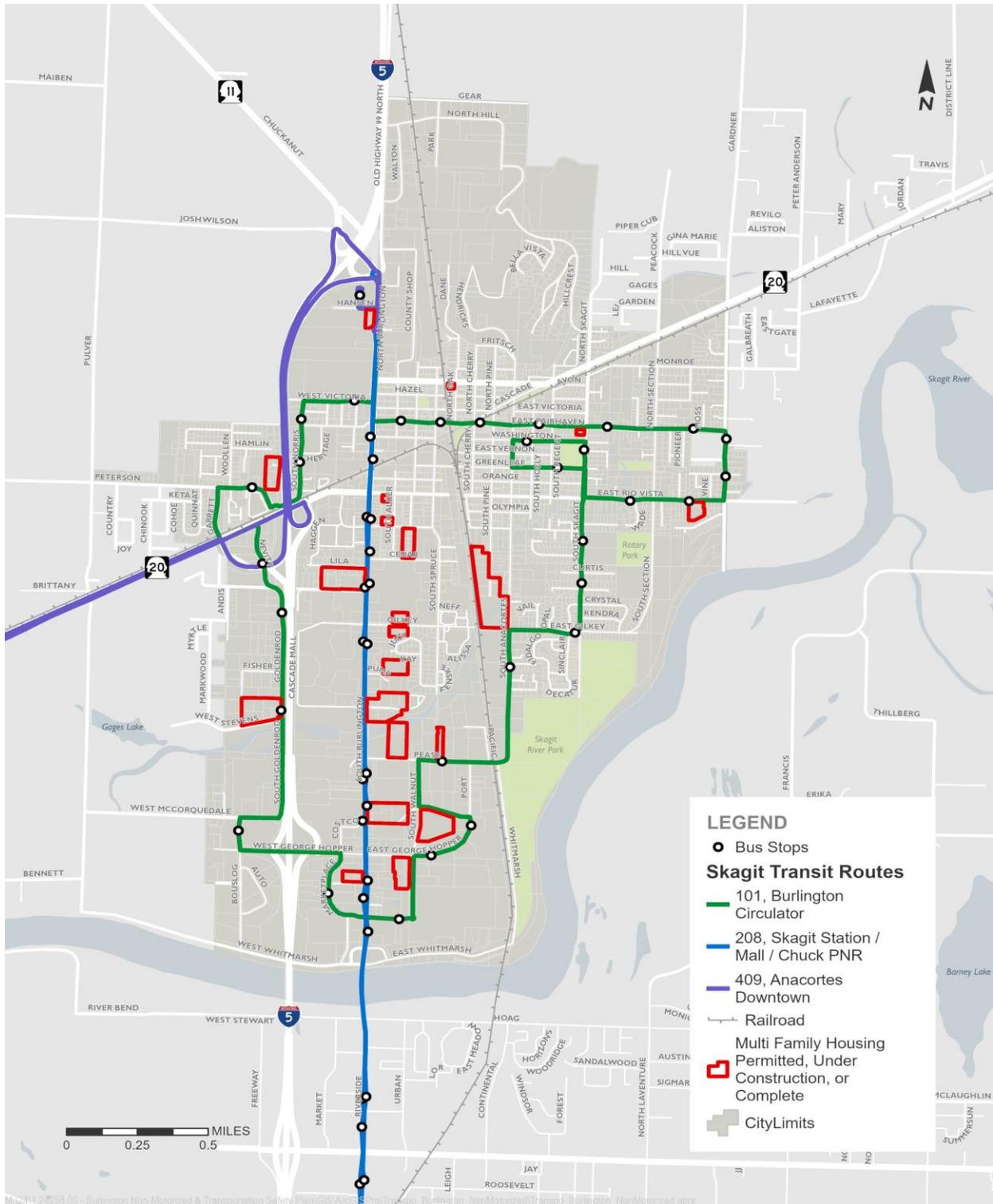


Figure 2.2. Figure 2.2. Multifamily Developments Approved by City of Burlington, 2017-2025

Who is Burlington Planning For?

A person's choice to walk, bike, roll, or ride transit is primarily influenced by the availability of well-connected sidewalks, bikeways, ADA ramps, crosswalks, streetlights, and user perception of safety and risk of conflict. People walking, biking, and rolling in wheelchairs and mobility devices and are the most **vulnerable road users** of the transportation system because they:

- Do not have a vehicle structure to protect them
- Are smaller and lighter than larger and heavier vehicles
- Travel at slower speeds (10-15 mph) than vehicles
- Are exposed to vehicle passing and turning conflicts
- Are at higher risk of injury in collisions with vehicles



Figure 2.3. Transportation User Scale of Vulnerability to Risk and Injury

Complete Streets

The foundational principle of the national Complete Streets movement is that all mobility needs, modes, land use contexts, and funding capacities must be carefully considered, balanced, and implemented for the multimodal transportation system to provide space and safety for everyone, where feasible. Not every street can have facilities for every user group, which means that priorities need to be established, trade-offs recognized, and decisions must be based on practical realities.

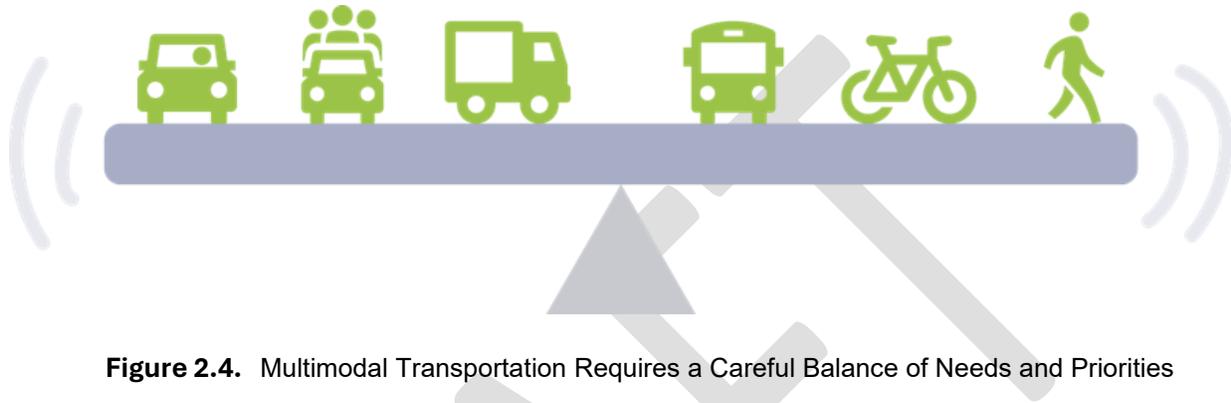


Figure 2.4. Multimodal Transportation Requires a Careful Balance of Needs and Priorities

Policy into Action

The City of Burlington was a relatively early adopter of a Complete Streets policy shortly after the State of Washington passed [RCW 47.04.320 Complete streets grant program](#) in 2011. This legislation encouraged local governments to adopt their own complete streets ordinances that would provide safe access to all “including bicyclists, pedestrians, motorists, and public transportation users.” This legislation also requires WSDOT to consult with local jurisdictions and consider the needs of all users by applying context-sensitive solutions when constructing, reconstructing, or making major improvements to streets that are part of the state highway system.

Burlington City Council Ordinance 1792 was adopted on November 26, 2013 and created [Burlington Municipal Code Title 12.26 Complete Streets](#), which reads as follows:.

12.26.010 Complete Streets

The purpose of the complete streets program is to plan for, design and construct all city transportation projects to provide appropriate accommodation for bicyclists, pedestrians, transit users and persons of all abilities in comprehensive and connected networks.

12.26.020 Exceptions

Facilities for bicyclists, pedestrians, transit users and/or people of all abilities are not required to be provided:

- A. Where their establishment would be contrary to public safety;
- B. Where there is no identified long-term need; or
- C. Where the public works director grants a documented exception which may only be authorized in specific situations where conditions warrant. Such site-specific exceptions shall not constitute general changes to the standards set in BMC § 12.26.010.

Per BMC 12.26.020 B, this Active Transportation and Safety Plan identifies long-term pedestrian and bicycle infrastructure needs on a citywide Active Transportation Network.

Level of Traffic Stress

As mentioned above, traffic volume, speed, and physical separation are the roadway variables that determine whether a person walking, biking, or rolling feels safe and comfortable. These variables, applied to the Active Transportation Network, establish a measure of **Level of Traffic Stress (LTS)**. As the volume, speed, and proximity to traffic decreases, so does the level of stress that a person walking, biking, or rolling feels while riding along a roadway. A walking or biking facility physically separated from vehicle traffic has the lowest stress of all. LTS is not part of the MMLOS standard for walking and biking facilities but is intended for use as a measure of user experience to help with the prioritization of active transportation investments on Burlington’s citywide multimodal surface transportation system during the annual six-year TIP process.



Figure 2.5. Bicycle LTS = Measure of User Comfort

- Based on age, physical health, and confidence
- Subjective to individual user experience
- Wide spectrum of user skill levels

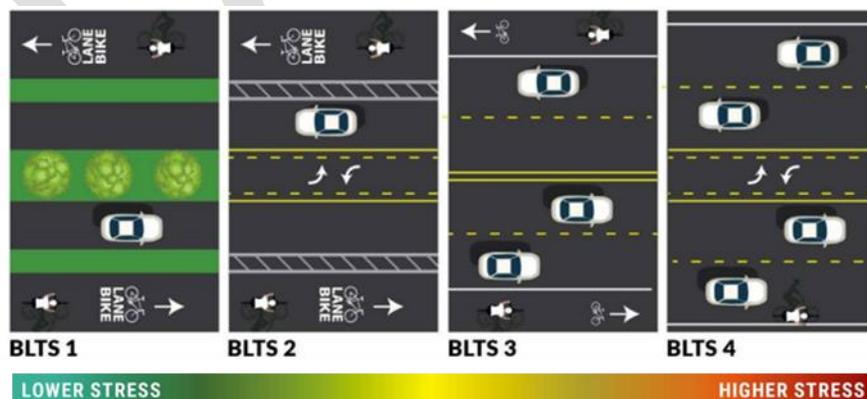


Figure 2.6. Bicycle LTS = Measure of Facility Comfort

- Based on facility and user proximity to moving traffic, speed, volume, land use context

- Subject to physical space (ROW) available and agency financial constraints

Chapter 3 - Active Transportation Network

The 2025 Burlington Active Transportation and Safety (ATS) Plan meets current policy direction from the GMA by examining existing conditions, considering the needs of future growth and development, establishing an active transportation network, MMLOS standards, and a process to track and monitor progress made toward network completeness. For active transportation, MMLOS standards focus on network connectivity and completeness rather than design capacity or user counts while performance and prioritization measures focus on safety and comfort. The ATS also promotes methods to reduce vehicle miles traveled (VMT) and greenhouse gas (GHG) emissions, consistent with GMA and the Climate Element of the Burlington Comprehensive Plan.

Establishing an **Active Transportation Network (ATN)** within an auto-dominated environment can be challenging in a small urban area. The land use context and popular destinations often determine what type of walking and bicycle facilities might be needed and can be provided. The first step is to take an inventory of regional walking, biking, and multiuse facilities that currently exist. The next step is to examine the physical space and constraints of the existing citywide street system to determine if there may be opportunities to enhance or repurpose what already exists, such as removal or reconfiguration of on-street parking to add bike lanes, restriping streets to add bicycle lanes, or rechannelization of a 4-lane street to a 3-lane street with bike lanes, also known as a “road diet.” On low-volume streets, or on short street segments where there is no physical space, on-street shared lane markings (SLMs), also known as “sharrows,” can help to alert drivers to the presence of people on bikes and to help position bicyclists to share lanes with vehicles.

The ATS Plan establishes a long-term citywide ATN of existing and planned designated walking and bicycle facilities. The portion of the ATN that can realistically be constructed over the 20-year planning period will depend on the amount of funding available for recommended improvements on City streets, as well as WSDOT funding for improvements to SR 11 and 20.

Pedestrian and Bicycle Facility Maps and Project Tables

The ATN maps in Figure 3.1 and Figure 3.2 establish citywide networks of walkways and bikeways, some with dedicated facilities, and some without. Consistent with Burlington’s adopted MMLOS standards for active transportation, each ATN segment can be measured as complete, partially complete, or incomplete according to City or WSDOT roadway standards for walking and biking facilities, as shown in Figure 3.3 and Figure 3.4. These MMLOS status maps then translate to pedestrian and bicycle improvement needs shown in Figure 3.5 and Figure 3.7.

The ATN MMLOS maps can be updated each year and included in an **Annual Concurrency Report** to provide a baseline measure for the City of Burlington to consider funding opportunities to upgrade or complete missing ATN segments in the six-year TIP.

Evaluation of Citywide Pedestrian Network

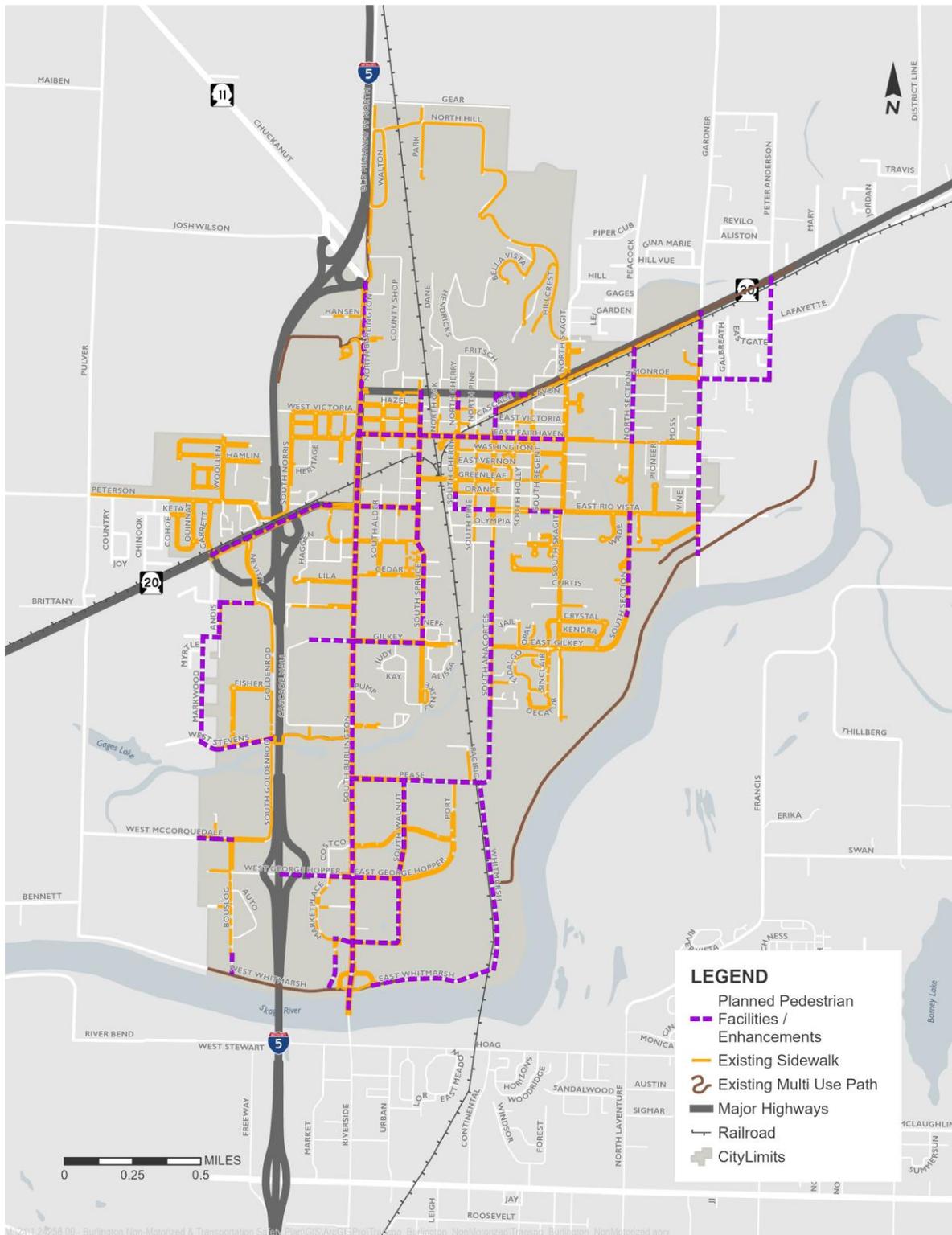


Figure 3.1. Active Transportation Network – Existing and Planned Pedestrian Facilities

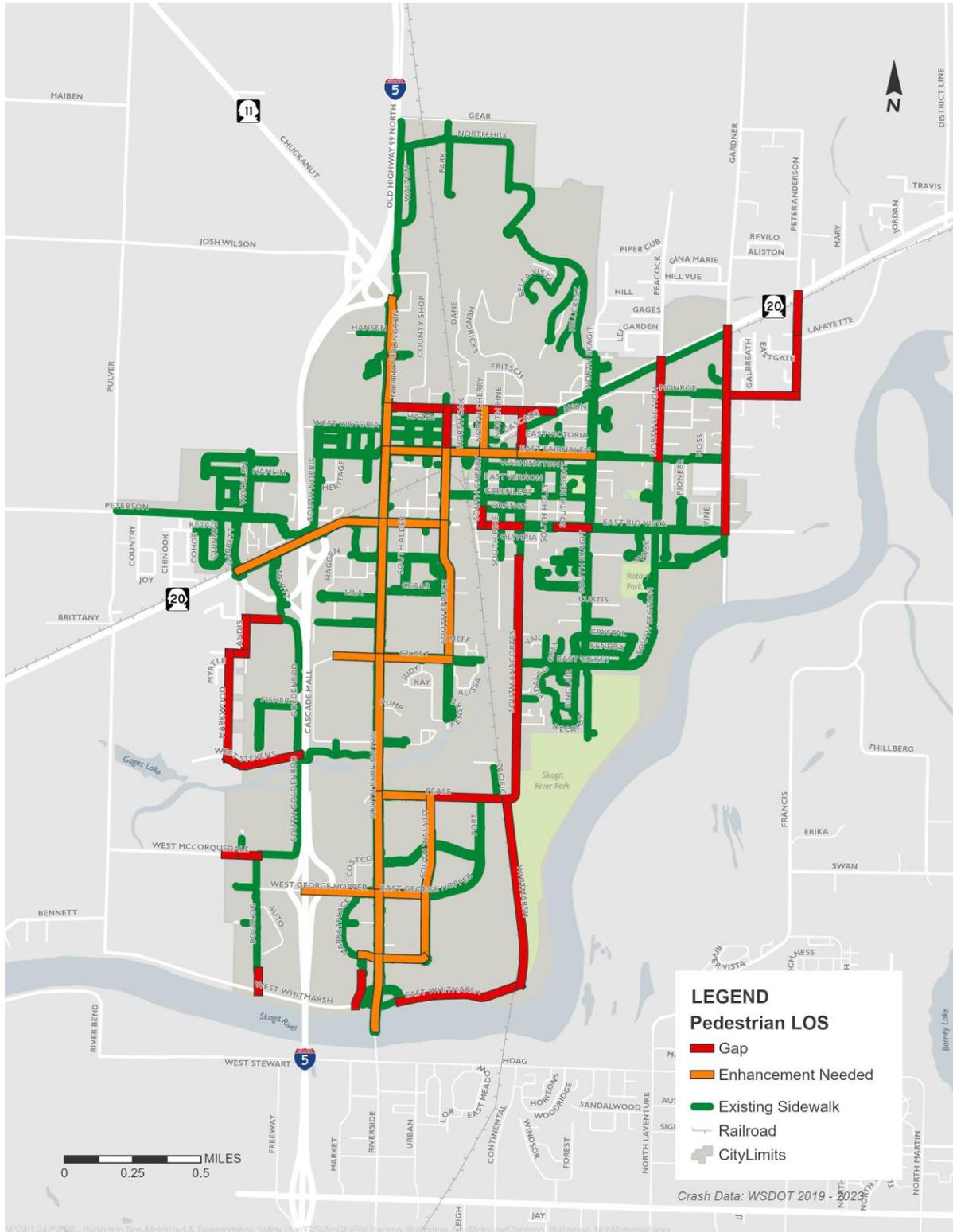


Figure 3.2. 2025 MMLOS Status of Pedestrian Active Transportation Network

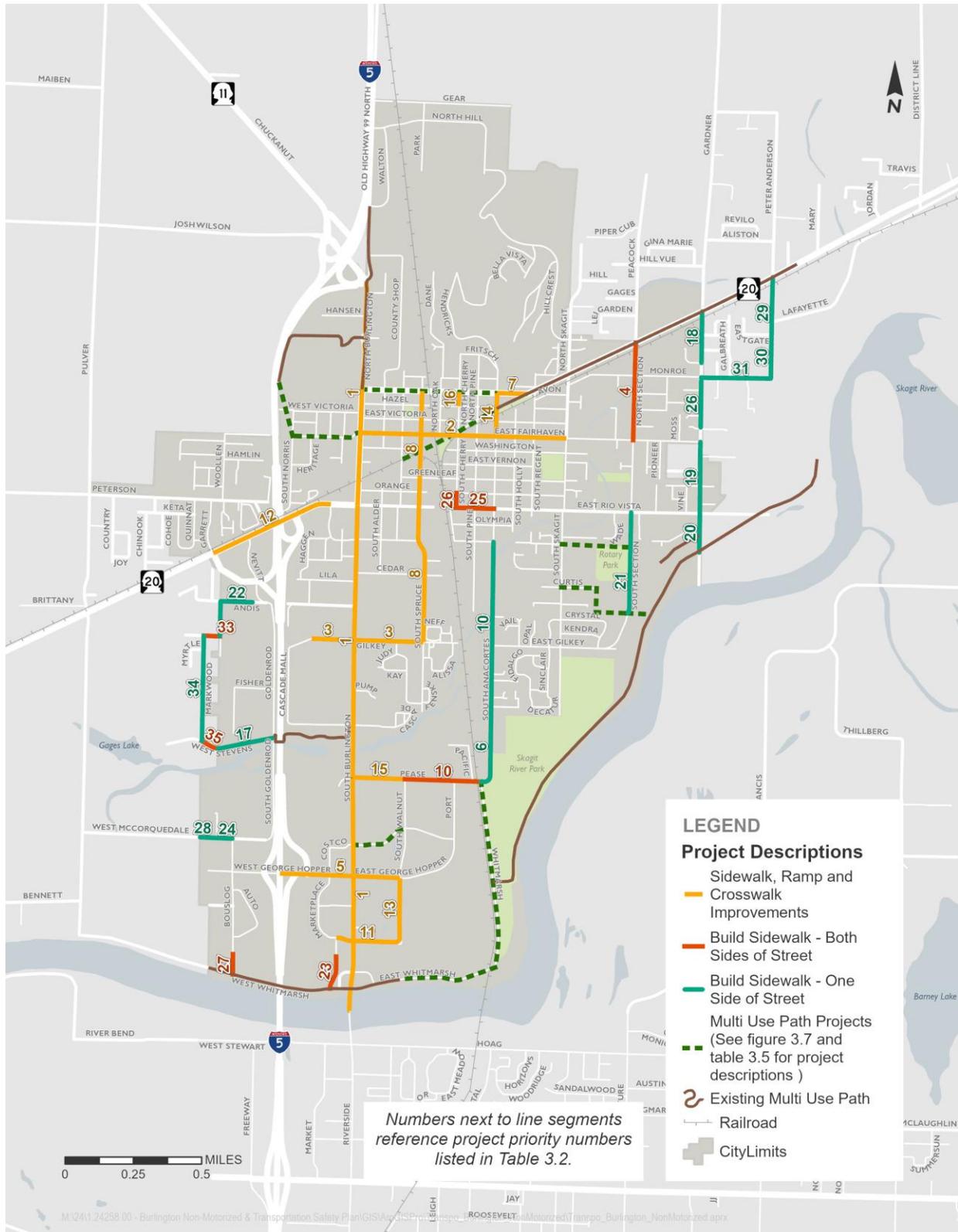


Figure 3.3. Pedestrian Active Transportation Network Projects

Pedestrian Scoring Criteria and Prioritization

To be effective, Active Transportation Networks (ATN) need to be cohesive and well-connected. Each ATN pedestrian segment was evaluated, scored, and prioritized using a variety of safety, equity, and land use-transportation context and connectivity criteria, such as:

- **Safety** - Pedestrian crash history multiplied by number and severity of crashes
- **Equity** – According to the equity analysis detailed in Appendix B
- **Recent Multifamily Housing** – Connectivity and proximity (within ¼-mile) to sidewalks, bikeways, and transit service according to City information on development since 2016
- **School or Daycare** – Connectivity and proximity (within ½-mile) to Elementary, Middle, or High Schools, as well as Daycare establishments
- **Park or Trail** – Connectivity and proximity (within ¼-mile) to public parks or trails
- **Transit or Bike Lanes** – Served by a Skagit Transit route or connectivity and proximity (within ¼-mile) to other bike lanes or trails
- **Crossing of I-5, SR 20, or Railroad tracks** – Need for crossing of an active transportation mobility barrier
- **Institutional-Commercial Destination** – Connectivity and proximity (within ½-mile) to post office, grocery store, medical and social services, etc.
- **Level of Traffic Stress** – according to vehicle traffic volume, speed, and degree of separation
- **Total Score** = Cumulative total of points scored for all factors listed above

Table 3.1.

Table 3.1. Scoring Criteria and Points for Citywide Pedestrian Network									
Safety [Ped Crash] (3)	Equity (5)	Recent Multi Housing (2)	School or Day Care (2)	Park or Trail (2)	Transit or Bike Lane (2)	Crossing of I-5, SR 20, or RR Tracks (2)	Institutional-Commercial Destination (2)	Level of Traffic Stress Rank (3)	Total (Up to 24)

Scores and Priorities

This evaluation provides an objective assessment of the value and benefit of pedestrian improvements on one ATN segment **relative to** other segments across the entire City of Burlington. Each ATN segment was scored using individual land use and transportation criteria listed above and then a cumulative score was derived for each ATN segment to provide a list of relative priorities. It should be recognized that this represents a ‘moment in time’ analysis and evaluation based on existing (2025) conditions. As Burlington continues to grow and change, this evaluation should be repeated with each 10-year Comprehensive Plan update cycle, or more frequently if the City has the staff, resources, and budget to do so.

The improvement projects listed in Table 3.2 are not intended to be constructed in scored or ranked order. To be most cost-effective, construction will depend on several factors, including timing, funding availability, demand created by development, and extending street frontage improvements made by private development.

Recommended Pedestrian Improvements

Pedestrian facility improvement recommendations are made for each individual ATN segment. In an urban area like Burlington, safety will be increased and both citizens and visitors will benefit from the presence of ADA-compliant sidewalks, curb ramps, and crosswalks. This is particularly important on downtown streets where the City is trying to focus on economic development and mobility, but also in between the residential areas where people live and the daily destinations that they may wish to walk to from their homes, such as schools, parks, stores, medical services, and work, etc.

Pedestrian Facility Improvement Types

Every trip involves at least a small amount of pedestrian travel whether walking to or from a vehicle parking space, a bicycle rack, or a transit stop. Pedestrian improvements are required for every public street frontage, whether funded by private developers, local public funds, or state and federal grant funds. Pedestrian improvement recommendations are primarily based on ADA needs, right-of-way width and availability, land use context (residential, commercial, industrial; high-density or low-density, etc.), transit service availability, and connectivity to popular community destinations.

Pedestrian facility construction costs will vary widely depending on timing, facility type, and any mitigation requirements, such as right-of-way acquisition or mitigation for environmental impacts and storm water treatment. For the recommendations listed in the tables below, planning level cost estimates are provided based on costs for similar facilities listed in the 2024 Bellingham Bicycle Master Plan. Pictures and descriptions of each bicycle facility type recommended are provided on the next page.

It should be noted that there are some locations in the tables that are listed as “Further Study Needed” (FSN) due to a lack of physical street width, lack of right-of-way available, low probability of street reconfiguration for bike facilities, etc. One example of this is Burlington Boulevard, which calls for sidewalk “enhancement” and/or widening. To be clear, all new developments abutting Burlington Boulevard is subject to street frontage standards listed in BMC 12.28.010 with subsections 120 - 160 specifying requirements for each street classification. In addition, streets categorized as “Pedestrian amenity streets” require different sidewalk widths and drive through uses are required to include "enhanced pedestrian amenities" (BMC 17.50.070.C.7). However, if the desire is to program public funding for a comprehensive enhancement/widening of sidewalks along the full length of Burlington Boulevard, then an engineering feasibility study would be required to assess challenging factors, such as lack of physical space or right-of-way, environmental impacts, mitigation requirements, and associated costs for construction.

Table 3.2 below shows how various ATN segments shown on Figures 3.1, 3.2, and 3.3 were evaluated and scored for future pedestrian network improvements.

Pedestrian Facility Illustrations and Descriptions



ADA Curb Ramps = Required by the American with Disabilities Act (ADA) of 1990. Provide a gentle slope transition from elevated sidewalks to street surfaces with mats and truncated domes that allow detection of a change in travel conditions by vision-impaired travelers.
Application: Located on intersection corners, mid-block crossings, and center island pedestrian refuges.



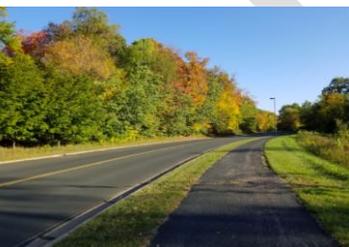
ADA Sidewalk = Minimum 5' wide with 48" unobstructed level surface typically raised above street level and separated by a 6" curb and gutter system to capture and drain storm water.
Application: Almost all public streets except where exceptions have been made or where not warranted by low traffic volume and speed conditions.



Curb-Separated Walkways = Located at street edge, fully separated from vehicle traffic by a protective curb barrier; Minimum 5-foot width for one-way; Minimum 10-12-foot width for two-way; Requires special treatment at intersections.
Application: Used on one or both sides of suburban streets with higher volume/speed as an alternative to raised concrete sidewalks.



Setback Sidewalks with Street Trees or Landscape Strips = Concrete sidewalks separated from roadway by a 4-foot wide landscape strip, street trees, or other landscaping vegetation.
Application: Used on one or both sides of urban arterial streets to separate pedestrians from higher volume/speed vehicle traffic. Improves safety, aesthetics, shade, potential traffic calming.



Paved Multiuse Pathways = Located on one side of street (Or thru Park property or on top of river dike); Physically separated from roadway by landscape strip of street trees (or elevated on top of river dike); Minimum 10-12-foot width for two-way for two-way travel; ADA-compliant, shared use by people walking, biking, and rolling.
Application: Anywhere that right-of-way or public use easement allows. Maximum safety and comfort

Figure 3.4. Pedestrian Facility Type Illustrations, Descriptions, and Applications

Pedestrian Crossing Illustrations and Descriptions



ADA Crosswalks = Provides clearly identifiable markings across streets for dedicated pedestrian space where drivers are required to grant right-of-way to people walking and rolling. Accompanied by signs mounted on posts; Retroreflective paint commonly used.

Application: Primarily at arterial street intersections or mid-block crossings, but also sometimes across wide commercial driveways.



High-Visibility Crosswalks = Use patterns (i.e., bar pairs, continental, ladder) visible to both the driver and pedestrian from farther away compared to traditional transverse line crosswalks. Often include advance high-visibility signs and street markings.

Application: Midblock pedestrian crossings and uncontrolled intersections, as well as local school and park locations.



Rectangular Rapid Flashing Beacon (RRFB) = These user-activated signals consist of two, rectangular- shaped yellow indications, each with a light-emitting diode (LED)-array-based light source. RRFBs flash with an alternating high frequency when activated to enhance conspicuity of pedestrians at the crossing to drivers.

Application: Particularly effective at two-lane crossings with speed limits less than 35 miles per hour, local school, and park locations.



Pedestrian Hybrid Beacon (PHB) or High-Intensity Activated Crosswalk (HAWK) Signal = User-activated overhead signal to alert drivers and help pedestrians safely cross higher-speed roadways at midblock crossings and uncontrolled intersections. The signal beacon head consists of two red lenses above a single yellow lens.

Application: Multilane pedestrian crossings with insufficient gaps, speeds above 35 mph, or corridors with higher traffic volumes.



Traffic Signal with Lead Pedestrian Interval (LPI) Phase = Gives pedestrians the opportunity to enter the crosswalk at an intersection 3-7 seconds before vehicles are given a green indication to better establish presence in the crosswalk before vehicles turn right or left.

Application: LPI is recommended for use at intersections with high pedestrian presence and/or high turning vehicle volumes, such as a downtown corridor or shopping area.

Figure 3.5. Pedestrian Crossing Type Illustrations, Descriptions, and Applications

Table 3.2. Priority Scores and Plan-Level Cost Estimates for Citywide Pedestrian Network										
Priority Rank	Segment Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Total Score
1	Burlington Blvd	Fountain St	Skagit River	Principal	50' avg.	Further Study Needed	Enhance/Widen Sidewalks, ADA, and crosswalks	10,560	\$75,000	87
2	Fairhaven Ave	Burlington Blvd	Skagit St	Connector	50' at crossing		(9) Intersection Curb Extensions, ADA ramps, crosswalks	3,930	\$1,440,000	34
3	Gilkey Rd	Burlington Gallery (Mall)	S Spruce St	Connector	35'		Mid-Block RRFB Crosswalk w center refuge, ADA upgrade	2,100	\$100,000	31
4	N. Section St	SR 20	E. Fairhaven St		23'		Sidewalk 2 sides (Or Paved Multiuse Pathway 1-side)	1,865	\$2,480,000	31
5	George Hopper Rd	I-5	S Walnut St	Minor	65'	Further Study Needed	Enhance/Widen Sidewalks, ADA , and crosswalks	2,330	\$75,000	21
6	S. Anacortes St	E. Whitmarsh Rd	Cul-de-Sac		36'		Sidewalk eastside	1600	\$1,064,000	21
7	Avon Ave (SR 20)	N. Anacortes Street	Regent St	Principal	22'		Multi-Use Path	3,320	\$250,000	20
8	N Spruce St	E Magnolia Ave	Gilkey Rd	Connector	37'		Intersection Curb Extensions (9), ADA ramps, crosswalks	5,140	\$1,500,000	18
9	S. Anacortes St	E. Whitmarsh Rd	E. Sharon Place		36'		Sidewalk westside	4,650	\$3,092,250	18
10	Pease Rd	S. Walnut St	E. Whitmarsh Rd		40'		Sidewalk 2 sides	1,600	\$2,128,000	15
11	Marketplace Dr	S Burlington Blvd	S Walnut St	Local	34'		Sidewalk north side	1,190	\$791,350	14
12	SR 20/W Rio Vista	Garrett Rd	Haggen Drive	Principal	101'	Further Study Needed	Improved Sidewalks, ADA ramps, crosswalks	2,440	\$50,000	14
13	S Walnut St	George Hopper Road	Marketplace Dr	Local	22'		Multiuse Pathway west side, ADA ramps, crosswalks	1,160	\$476,760	13
	Sheet 1 Subtotal								\$13,522,360	

Table 3.2.

Priority Rank	Segment Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Total Score
14	Anacortes Street	SR 20	Fairhaven Ave	Local	23'		Sidewalk 2 sides, ADA ramps, crosswalks	838	\$1,115,000	13
15	Pease Rd	S Burlington Blvd	S Walnut St	Connector	40'		ADA ramps, crosswalks	975	\$50,000	13
16	N Cherry St	SR 20	E. Hazel Avenue	Local	35'		Sidewalk east side, ADA ramps, crosswalks	250	\$200,000	10
17	Stevens Rd	City-UGA Boundary	S. Goldenrod Rd				Sidewalk south side	1,075	\$715,000	10
18	Gardner Rd	SR 20	Poplar Place		32'		Sidewalk westside	900	\$599,000	9
19	Gardner Rd	E. Fairhaven Avenue	E Rio Vista Avenue		32'		Sidewalk westside	1,350	\$897,750	9
20	Gardner Rd	E Rio Vista Aveue	Multiuse Trail parallel to Skagit River		32'		Sidewalk westside	730	\$485,450	9
21	S. Section St	E. Rio Vista Ave	994 S Section St				Sidewalk eastside	1,950	\$1,296,750	9
22	Andis Rd	WSDOT driveway	Andis Place				Sidewalk northside	600	\$399,000	8
23	Marketplace Drive	Home Depot	E. Whitmarsh Rd				Sidewalk 2 sides	460	\$612,000	6
24	W. McCorquedale Rd	U-Haul driveway	Bouslog Rd				Sidewalk south side	600	\$399,000	5
25	E. Rio Vista Ave	S. Anacortes St	Cherry St				Sidewalk 2 sides	680	\$904,000	4
26	Cherry St	E. Rio Vista Ave	E. Orange Ave				Sidewalk 2 sides	260	\$346,000	4
27	Bouslog Rd	Auto Dealerships	W. Whitmarsh Rd				Sidewalk 2 sides	380	\$505,000	4
28	W. McCorquedale Rd	Lindawood Lane	City-UGA Boundary				Sidewalk south side	330	\$219,000	3
	Sheet 2 Subtotal								\$8,742,950	
	Sheets 1 & 2 Grand Total								\$22,265,310	

Table 3.3. Priority Scores and Plan-Level Cost Estimates for Burlington UGA ¹ Pedestrian Network										
Priority Rank	Segment Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Total Score
29	Peter Anderson Rd (UGA)	SR 20	Lafayette Rd		21		Sidewalk 1-side	800	\$532,000	8
30	Lafayette Rd (UGA)	Peter Anderson Rd	90 degree curve at 20312 Lafayette				Sidewalk 1-side	1,065	\$710,000	8
31	Lafayette Rd (UGA)	90 degree curve	City limit at Gardner Rd				Sidewalk 1-side	1,325	\$880,000	8
32	Andis Rd (UGA)	Andis Place	90 degree curve				Sidewalk westside	670	\$446,000	5
33	Andis Rd (UGA)	90 degree curve	Markwood Rd				Sidewalk 1-side	300	\$200,000	5
34	Markwood Rd (UGA)	Andis Rd	W. Stevens Rd				Sidewalk 1-side	2,000	\$1,330,000	5
35	W. Stevens Rd (UGA)	Markwood Rd	City-UGA Boundary				Sidewalk 2 sides	340	\$226,000	5
	Sheet 4 Subtotal								\$4,324,000	
Note: Skagit County responsibility until annexation to City. Potential opportunity for City-County fundig partnership.										

Table 3.3.

Figure 3.6. Burlington Active Transportation Network – Planned Bicycle Facilities

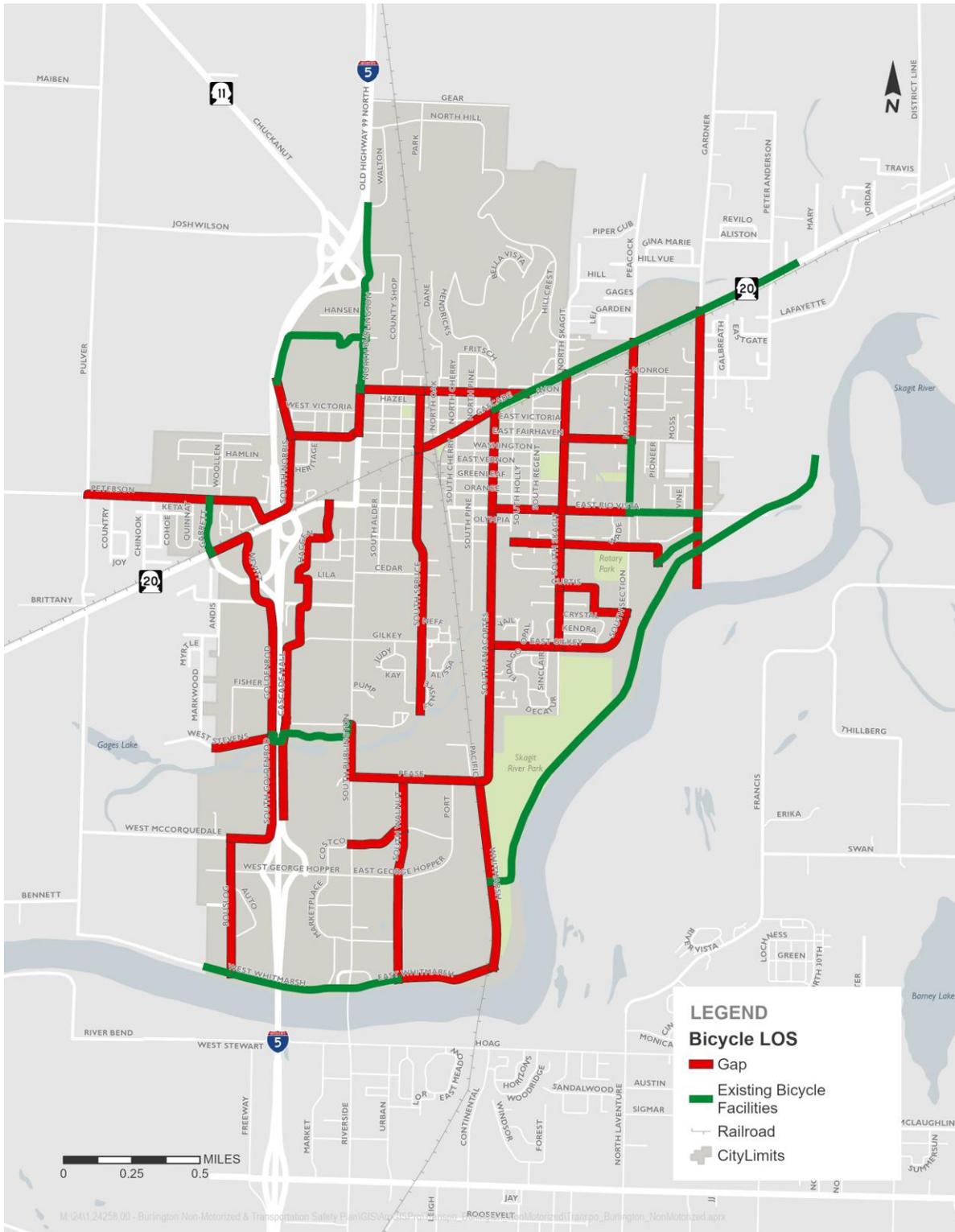


Figure 3.7. 2025 MMLOS Status of Bicycle Active Transportation Network

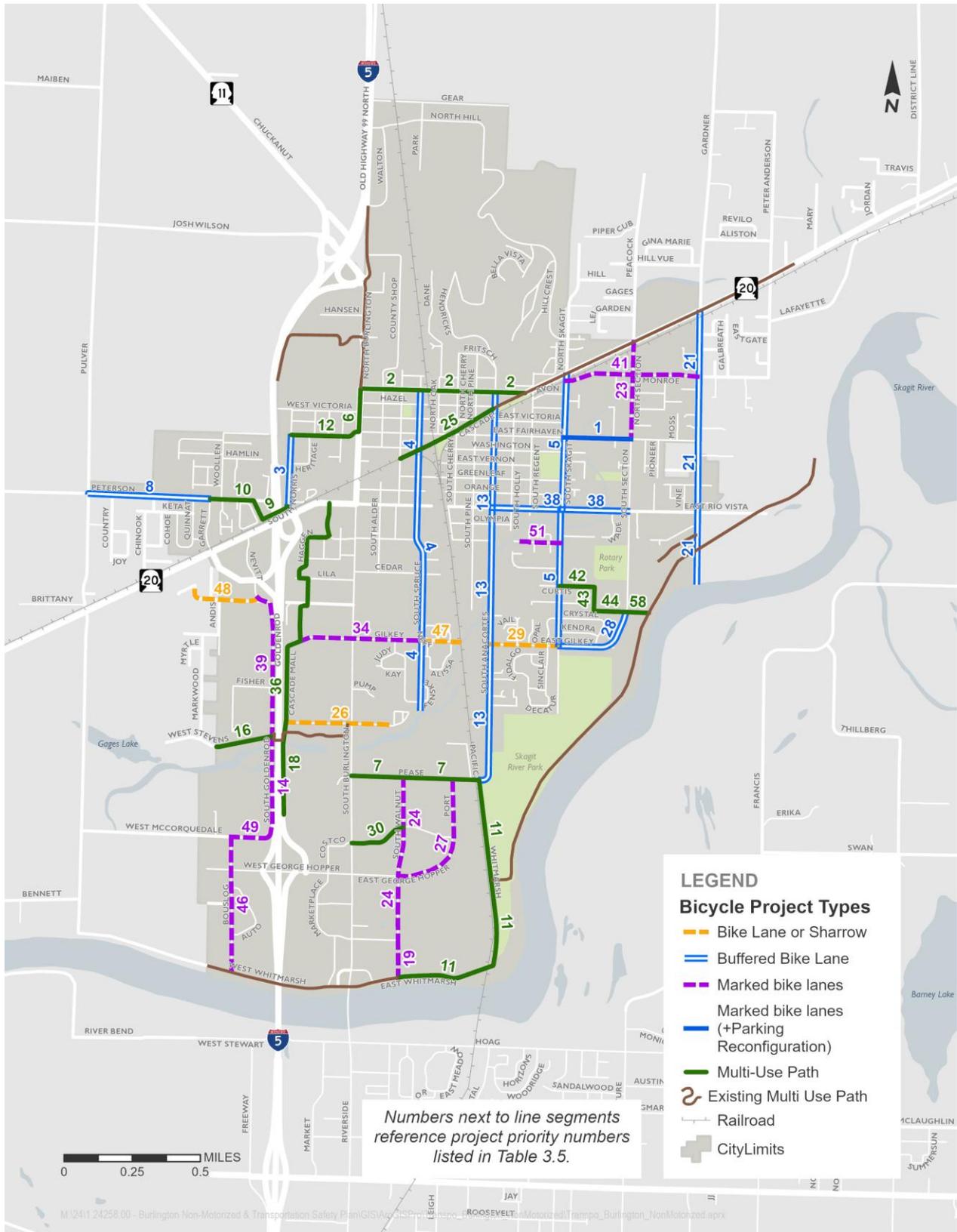


Figure 3.8. Bicycle Active Transportation Network Projects

Bicycle Scoring Criteria and Prioritization

To be effective, Active Transportation Networks (ATN) need to be cohesive and well-connected. Each ATN bicycle segment was evaluated, scored, and prioritized using a variety of safety, equity, and land use-transportation context and connectivity criteria, such as:

- **Safety** - Bicycle crash history multiplied by number and severity of crashes
- **Equity** – According to the equity analysis detailed in Appendix B
- **Recent Multifamily Housing** – Connectivity and proximity (within ¼-mile) to sidewalks, bikeways, and transit service according to City information on development since 2016
- **School or Daycare** – Connectivity and proximity (within ½-mile) to Elementary, Middle, or High Schools, as well as Daycare establishments
- **Park or Trail** – Connectivity and proximity (within ¼-mile) to public parks or trails
- **Transit or Bike Lanes** – Served by a Skagit Transit route or connectivity and proximity (within ¼-mile) to other bike lanes or trails
- **Crossing of I-5, SR 20, or Railroad tracks** – Need for crossing of an active transportation mobility barrier
- **Institutional-Commercial Destination** – Connectivity and proximity (within ½-mile) to post office, grocery store, medical and social services, etc.
- **Level of Traffic Stress** – according to vehicle traffic volume, speed, and degree of separation
- **Total Score** = Cumulative total of points scored for all factors listed above

Table 3.4.

Table 3.4. Scoring Criteria and Points for Citywide Bicycle Network									
Safety [Bike Crash] (3)	Equity (5)	Recent Multi Housing (3)	School or Day Care (2)	Park or Trail (2)	Transit or Bike Lane (2)	Crossing of I-5, SR 20, or RR Tracks (2)	Institutional- Commercial Destination (2)	Level of Traffic Stress Rank (3)	Total Score

This evaluation provides an objective assessment of the relative value and benefit of bicycle improvements on one ATN segment relative to other segments across the entire City of Burlington. Each ATN segment was scored using individual land use and transportation criteria listed above and then a cumulative score was derived for each ATN segment to provide a list of relative priorities. It should be recognized that this represents a ‘moment in time’ analysis and evaluation based on existing (2025) conditions. As Burlington continues to grow and change, this evaluation should be repeated with each 10-year Comprehensive Plan update cycle, or more frequently if the City has the staff, resources, and budget to do so.

Recommended Bicycle Improvements

Bicycle facility improvement recommendations are made for each individual ATN segment, but the City should always strive to balance various priorities on different streets and land use contexts. As an example, downtown streets focus on both economic development and mobility, but the City will need to decide if angled parking can, or should, be reconfigured to parallel parking to allow the installation of bike lanes. On other arterial streets, the City may need to decide to retain or remove on-street parking in favor of installing bike lanes or on-street multiuse pathways. This is covered in more detail in the implementation section below.

Bicycle improvement recommendations are based on a wide variety of factors, including, but not limited to physical street width between curbs, right-of-way width and availability, presence of on-street parking and potential for removal on one or both sides, relative traffic volume, posted speed limit, land use context (residential, commercial, industrial; high-density or low-density, etc.), and connectivity to popular community destinations.

It should be noted that there are some locations in the tables that are listed as “Further Study Needed” (FSN) due to a lack of physical street width, lack of right-of-way available, low probability of street reconfiguration for bike facilities, etc. One example of this is Nevitt Road from SR 20 to Goldenrod Road. Immediately south of SR 20 to the north driveway to McDonalds fast food restaurant, the physical road width is consumed with multiple vehicle lanes between curbs. South of this, a center turn lane exists to accommodate left-turns into the Holiday Sports store and the south driveway to McDonalds. South of this, a center turn lane is provided to the Pacific Pride fuel station driveway. While there are currently no driveways between McDonalds and Pacific Pride, it is possible that future development will create new driveways, and the presence of the center turn lane will provide a safety benefit. South of Pacific Pride, the center turn lane continues around the curve and provides a safety benefit for left turns onto Andis Road, Hugo Helmer Music, and Rodda Paint before tapering off onto Goldenrod Road to the south. While it may be possible to remove the center turn lane south of McDonalds in favor of installing marked bike lanes, this may increase safety risk for angle (left turn) and rear-end crashes to vehicle drivers. Currently, the removal of the center turn lane is not recommended.

Bicycle Facility Improvement Types

There are a wide variety of bicycle improvement options and construction costs will vary widely depending on timing, facility type, and any mitigation requirements, such as right-of-way acquisition or mitigation for environmental impacts and storm water treatment. For the recommendations listed in the tables below, planning level cost estimates are provided based on costs for similar facilities listed in the 2024 Bellingham Bicycle Master Plan. Pictures and descriptions of each bicycle facility type recommended are provided on the next page.

Table 3.5 below shows how various ATN segments shown on Figures 3.6, 3.7, and 3.8 were evaluated and scored for future bicycle network improvements.

Bicycle Facility Illustrations and Descriptions



Shared Lane Markings (SLMs) = Located in vehicle lanes to position bicyclists and to alert drivers that bicyclists may be present and that lane should be shared; Bicycle symbol and multi-chevron markings on street; Possible wayfinding; Not dedicated bicycle facility space.
Application: Local access streets < 2,500 ADT; Only on used arterials to fill gaps between dedicated bicycle facilities or on downhills in conjunction with uphill dedicated climbing lanes.



Marked Bike Lanes = Located curbside on both sides of street; Minimum 5-foot wide; Dedicated space with bicycle markings and arrows on street and signs mounted on posts; Retroreflective edgelines 4" to 8" width.
Application: Arterial streets >2,500+ average daily traffic (ADT); Posted speed 25-45 mph, but need for separation increases with speed, where feasible.



Buffered Bike Lanes = Located against curb or against on-street parking; Minimum 5-foot wide, plus 1 to 2-foot wide hatch-mark buffer against parked vehicles OR against higher speed vehicle lanes.
Application: Arterial streets >2,500+ average daily traffic (ADT); Posted speed 25-45 mph, but buffer width separation should increase with speed, where feasible.



Protected (Separated) Bike Lanes = Located at street edge, fully separated from vehicle traffic by a protective vertical barrier; Minimum 5-6-foot width for one-way; Minimum 10-12-foot width for two-way; Requires special treatment at intersections.
Application: Used on high-volume arterial streets with higher speeds or many turning conflicts, such as commercial retail driveways.



Paved Multiuse Pathways = Located on one side of street (Or thru Park property or on top of river dike); Physically separated from roadway by landscape strip of street trees (or elevated on top of river dike); Minimum 10-12-foot width for two-way for two-way travel; ADA-compliant, shared use by people walking, biking, and rolling.
Application: Anywhere that right-of-way or public use easement allows. Maximum safety and comfort

Figure 3.9. Bicycle Facility Type Illustrations, Descriptions, and Applications

Table 3.5. Priority Scores and Plan-Level Cost Estimates for Citywide Bicycle Network										
Priority Rank	Street Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	2016 Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Total Score
1	E Fairhaven Ave	S Skagit St	S Section St	Connector	32'	None	Marked bike lanes (+Parking Reconfiguration)	6,550	\$1,100,000	22
2	Avon Ave (SR 20)	N Burlington Blvd	Regent St	Principal	22'	None	Multi-Use Path	3,320	\$1,364,520	16
3	S Norris St	South end of S Norris	W Fairhaven Ave	Connector	38'	None	Buffered Bike Lane	1,900	\$499,700	15
4	Spruce St	SR 20	Fenske Ln	Minor	33'	None	Buffered Bike Lane	6,072	\$1,596,936	14
5	Skagit St	Hwy 20	Gilkey Rd	Connector	38'	None	Buffered Bike Lane	5,270	\$1,386,010	13
6	N Burlington Blvd	Avon Ave	to W Fairhaven Ave	Principal	55'	None	Multi-Use Path	760	\$312,360	12
7	Pease Rd	S Burlington Blvd	E Whitmarsh Rd	Connector	30'	None	Multi-Use Path	2,270	\$932,970	12
8	Peterson Rd	Western City limit	Frontage Rd	Minor	35'	None	Buffered Bike Lane	3,140	\$825,820	12
9	S Norris St	Frontage Rd	South end of S Norris St east of I-5	Connector	20'	None	Multi-Use Path	580	\$825,820	12
10	Frontage Rd	Peterson Rd	S Norris St	Local	33'	None	Multi-Use Path	390	\$238,380	12
Sheet 1 Subtotal									\$9,477,076	

Table 3.5.

Segment Number	Street Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	2016 Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Priority Total
11	E Whitmarsh Rd	Pease Rd	Southwest City boundary	Connector	22'	None	Multi-Use Path	8,448	\$3,472,128	11
12	W Fairhaven Ave	S Norris St	Burlington Blvd	Connector	40'	None	Multi-Use Path	1,270	\$521,970	11
13	Anacortes Street	SR 20	S Whitmarsh Rd	Minor	28'- 36'	None	Buffered Bike Lane	7,392	\$1,944,096	11
14	Goldenrod Rd	W Stevens	Bouslog/McCorkdale		35	None	Marked bike lanes	1,975	\$312,050	10
15	SR 20	Garret	Nevitt	Principal	90'	Sidewalk on south side	Further Study Needed	732	\$50,000	10
16	Stevens Rd	W Stevens Rd	S Goldenrod Rd	Local	20'	None	Multi-Use Path	1,050	\$431,550	10
18	Gages Slough Trail Parallel I-5 Northbound	Gages Slough Trail	The Outlet Shoppes	Trail	n/a	None	Multi-Use Path	1,500	\$616,500	10
19	S Walnut St	Marketplace Drive	Whitmarsh		n/a	None	Marked bike lanes	740	\$116,920	10
20	Garrett Rd Bike Lane	SR 20	Peterson Road	Minor	5'	Bike Lane	n/a	1,056	n/a	10
21	Gardner Rd	SR 20	Skagit River	Minor	34'	None	Buffered Bike Lane	5,170	\$1,359,710	10
22	Nevitt Rd / S Goldenrod Rd / W McCorquedale Rd / Bouslog Rd	SR 20 west of I-5	W Whitmarsh Rd	Principal	34'	None	Buffered Bike Lane	10,032	\$2,638,416	9
23	Section St	HWY 20	E Fairhaven Ave	Connector	28'	None	Marked bike lanes	1,830	\$289,140	9
24	S Walnut St	Pease Rd	Marketplace Drive		32	None	Marked bike lanes	3,150	\$497,700	9
25	Cascade Hwy (SR 20)	Walnut St	to Anacortes St	Principal	30'	None	Multi-Use Path	2,040	\$838,440	9
26	Cascade Place	WSDOT I-5 ROW	Dead End				Shared lane Markings	2,500	\$5,000	9
27	E. George Hopper-Port Dr	S. Walnut Street	Pease Road				Marked bike lanes	2,500	\$395,000	8
Sheet 2 Subtotal									\$13,743,440	

Segment Number	Street Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	2016 Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Priority Total
28	S Section St	River Park access	Gilkey Rd	Connector	35'	None	Buffered Bike Lane	680	\$178,840	8
29	E Gilkey Rd	Anacortes St	S Section St	Connector	19'	None	Bike Lane or Sharrow	1,260	\$199,080	8
30	Path	S Walnut St/E McCorquedale Roundabout	S Burlington Blvd/Costco Dr intersection	n/a	25'	None	Multi-Use Path	1,336	\$549,096	8
31	Cascade Hwy (SR 20)	N Anacortes St	Avon Ave (e/o Regent St)	Trail	24	None	Multi-Use Path	600	\$246,600	8
32	Section St Bike Lane	E. Fairhaven Avenue	E. Rio Vista Avenue	Connector		Bike Lane	n/a	1,320	n/a	8
33	Rio Vista Ave Bike Lane	Section Street	Gardner Road	Minor		Bike Lane	n/a	1,320	n/a	8
34	Gilkey Road	WSDOT I-5 ROW	S. Spruce Street				Marked bike lanes and Shared Lane Markings	2,600	\$410,000	8
35	Nevitt Rd	SR 20	Goldenrod Rd		35	None	n/a	1,250	n/a	7
36	Haggen Dr	Through green areas to back side of Outlet mall		Local	28'	None	Multi-Use Path	6,125	\$2,517,375	7
37	Multiuse Pathway at Rotary Park	River Walk Lane	S Gardner Rd	Trail	n/a	None	Multi-Use Path	670	\$275,370	7
38	E Rio Vista Ave	Anacortes St	S Saction St	Minor	37'	None	Buffered Bike Lane	1,328	\$349,264	7
39	Goldenrod Rd	Nevitt/I5 underpass	W Stevens		35	None	Marked bike lanes	2,790	\$440,820	7
40	Old Hwy 99	Walton Drive	Gear Road				Paved Shoulder	2,000	\$284,000	7
41	Monroe St - Short St	Gardner Road	N. Skagit Street				Marked bike lanes and Shared Lane Markings	2,580	\$407,640	6
42	Curtis St	S Skagit St	Burlington Cemetery Dept Property	Local	30'	None	Multi-Use Path	640	\$263,040	6
43	Multiuse Pathway	West boundary of Burlington Cemetery Dept Property	to South Boundary	n/a	n/a	None	Multi-Use Path	480	\$197,280	6
Sheet 3 Subtotal									\$6,318,405	

Segment Number	Street Name/Location	From	To	Street Classification	Existing Width	2025 Existing Facility Type	2016 Proposed Facility Type	Linear Feet	Plan-Level Cost Estimate	Priority Total
44	Multiuse Pathway	South boundary of Burlington Cemetery Dept Property	S Section Street existing path	n/a	n/a	None	Multi-Use Path	605	\$248,655	6
45	Old Highway 99 North	Walton Drive	Gear Road	Trail	24	None	Multi-Use Path	2,000	\$822,000	6
46	Bouslog	W McCorquedale Road	Whitmarsh		28	None	Marked bike lanes	700	\$110,600	6
47	Gilkey Road	S. Spruce Street	BNSF ROW				Shared lane Markings	630	\$1,260	6
48	Andis Road	S. Goldenrod Road	Andis Place				Shared lane Markings	700	\$1,500	5
49	W McCorquedale Road	Goldenrod Rd	Bouslog Road		28	None	Marked bike lanes	700	\$110,600	5
50	Burlington Edison High School Trail					Multi-Use Path	n/a	3,696	n/a	5
51	E Sharon Ave	East end segment	Skagit St	Local	33'	None	Marked bike lanes	730	\$115,340	4
52	Sharon Street Path					Multi-Use Path	n/a	254	n/a	4
53	Tami Wilson Memorial Path/Gages Slough					Multi-Use Path	n/a	2,112	n/a	3
54	SR 20 Trail			Principal		Multi-Use Path	n/a	8,976	n/a	3
55	Dike Trail					Multi-Use Path	n/a	11,986	n/a	3
56	Walnut Pond Trail					Park Trail	n/a	1,056	n/a	1
57	Rivers Edge Trail					Multi-Use Path	n/a	1,742	n/a	1
Sheet 4 Subtotal									\$1,409,955	
Sheets 1-4 Grand Total									\$30,948,876	

Chapter 4. Funding and Implementation Strategies

Transforming Burlington’s citywide transportation system will require retrofitting existing auto-oriented roadways to multimodal urban street corridors with mobility amenities for all users, where feasible, over many years. Given the planning-level cost estimates for recommended pedestrian (\$22,265,310 City; \$4,324,000 UGA) and bicycle (\$30,948,976) improvement projects (\$66,632,090) in Chapter 3, it is critical to understand Burlington’s current and expected future financial resources and opportunities to implement the recommended projects.

Funding Mechanisms

Private Development Frontage and Mitigation

As development continues to occur, private developers will be required to fund and construct street frontage improvements, such as sidewalks and bikeways, on public streets according to the City’s street standards. This will help to fill gaps in existing sidewalk and bikeway networks. In addition, for larger or more impactful new development, the City may require a Transportation Impact Analysis (TIA) to determine if project impacts warrant any off-site improvements, such as intersections, traffic signals, or roundabouts. If so, then the City can require a proportionate share cost of the mitigation required as a condition of permit issuance.

Transportation Impact Fees

Transportation Impact Fees (TIF) are assessed on new development at the time of building permit issuance. TIFs are project-specific proportionate share contributions toward the overall cost of providing the transportation infrastructure deemed necessary to accommodate the level of planned growth in the Land Use Element of the Comprehensive Plan. Burlington’s growth strategy is to integrate land use and transportation planning with a focus on mixed-use infill development, which will allow people to live close to where they work, shop, play, and entertain themselves and their families. This will require complete and connected pedestrian and bicycle facilities, as recommended in Chapter 3. TIF revenue collected from new developments will be used to help fund the completion of the Active Transportation Network by leveraging other local funds and state and federal grant funds to construct sidewalks, crossings, bikeways, and multiuse paths.

Transportation Benefit District Funding

In 2024, the Burlington City Council approved a Transportation Benefit District (TBD) and on January 1, 2025, the City of Burlington Transportation Benefit District began collecting an additional one-tenth of one percent (.001) for sales and use tax within the City of Burlington. Local businesses are required to collect the appropriate new rate of sales tax for retail sales and services provided within the City of Burlington. The TBD sales tax revenue will be used to help fund transportation services and infrastructure, such as streets, traffic signals and roundabouts, sidewalks, and bikeways. This dedicated local funding source is approved for 10 years and can be renewed by the City Council. The City also has the option to increase the TBD sales tax percent in the future, but would be required seek voter approval for this in a local election.

Grant Funding

State and federal grants will need to provide the bulk of the funding necessary to complete the Active Transportation Network with recommended improvements listed in Chapter 3. The City may

need to take a more aggressive and opportunistic approach to grant writing or hire consultants to help maximize grant writing success. This also requires that the City has local match funds available and that the Public Works Department has staff capacity to administer and deliver grant-funded projects on time and on budget. Delayed or over budget projects usually result in grant agencies viewing the City as unreliable and not a good risk for future grant funding awards. A general schedule of grant funding sources, timing cycles, and opportunities is provided below.

Transportation Revenue Available for Improvements

Table 4.1.

Table 4.1 Options to Increase Revenue for Active Transportation			
	2014-2024		2025-2044
Source	Total	Annual Average	Recommended
Federal Grants¹	\$3,534,612	\$321,328	\$500,000/year
State Grants²	\$7,760,029	\$705,457	\$1,250,000/year
City TBD (new in 2025)	~	~	\$1,250,000/year
Transportation³ Impact Fees	\$2,512,116	\$228,374	Yet to be determined
Funding Partnerships⁴	~	~	Yet to be determined
NOTES			
1. Based on USDOT communications in 2025, it is uncertain whether some federal grants (SS4A) will be continued after 2026, but other federal grants are available.			
2. Burlington can seek reliable state grant funds from several TIB programs (UAP, AT, CS) and several WSDOT programs (SRTS, PBP, HSIP).			
3. Vehicle capacity only projects. New TIFs can include pedestrian, bicycle, and trail projects meant for commuting. This may have the effect of raising TIF base rates.			
4. Multi-agency opportunities such as Skagit County, Skagit Transit, Port of Skagit, WSDOT, Burlington-Edison School District, City of Mount Vernon, Skagit Valley Community College, etc.			

Effective and Affordable Strategies

There are many ways to implement changes and safety improvements that will benefit active transportation users. Table 4.1 provides a list of improvement options listed by transportation mode with relative costs expressed in the right-hand column. Many of the treatments listed can be implemented at low-cost with high-impact outcomes, while others require higher cost transportation infrastructure construction. One of the most effective, and transformative, active

transportation and multimodal safety measures available to any City is to reconfigure and repurpose existing streets from auto-oriented to multimodal. Where adequate physical space exists between curbs, this can be done by simply rechannelizing the lane lines on a street or by removing parking from one or both sides of a street to install marked or buffered bicycle lanes.

DRAFT

Table 4.2 Low-Cost, High-Impact Strategies for Multimodal Transportation Improvements		
\$ = Less than \$10,000 \$\$ = \$10,000-\$50,000; \$\$\$ = \$50,000 - \$100,000 \$\$\$\$ = \$100,000 - \$500,000 \$\$\$\$\$ = More than \$500,000		
Pedestrian	Scale	Relative Cost
Lead Pedestrian Interval (LPI) phase at traffic signals	Per Signal	\$
ADA curb ramp installation/upgrade	Per Ramp	\$
Curb extensions to increase pedestrian visibility and shorten crossings	Per Curb	\$
Center lane pedestrian safety refuges	Per Refuge	\$
High-visibility crosswalks (Signs, street markings, etc.)	Per Location	\$
Rectangular Rapid Flashing Beacon (RRFB) crosswalks (ADA-compliant)	Per Location	\$\$
Curb-separated alternate walkways on road shoulders	Per block	\$\$\$
Pedestrian Hybrid Beacon (PHB) crossing signals (ADA-compliant)	Per Location	\$\$\$\$
ADA-compliant concrete sidewalks with curbs, gutters, and storm water systems	Per block	\$\$\$\$
Bicycle		
Bicycle detection sensors at traffic signals	Per Signal	\$
Wider edge lines between vehicle and bicycle lanes	Per block	\$
Parking utilization study to justify on-street parking removal to install bike lanes	Per corridor	\$
Green bike boxes at traffic signals	Per Signal	\$\$
Green bike lanes at transition points (turn lanes, etc.)	Per Location	\$\$
Bicycle phase traffic signals	Per Signal	\$\$
Repurposing existing physical space between curbs to narrow vehicle lanes, narrow parking stalls, or elimination of parking for the inclusion of dedicated on-street bicycle lanes (AKA "Road Diet")	Per block	\$\$\$
Marked bicycle lanes (5' minimum width)	Per block	\$\$\$
Buffered bicycle lanes with vertical plastic reflective posts	Per block	\$\$\$\$+
Physical barrier (Concrete or planter box) protected bicycle lanes	Per block	\$\$\$\$
Separated off-street 10 to 12 feet wide paved two-way multiuse trails	Per block	\$\$\$\$
Transit		
Bus stop benches	Per Location	\$
Weather-protected transit shelter	Per Location	\$\$
Transit bus queue jump signal	Per Location	\$\$
ADA-compliant transit island for in-lane passenger boarding	Per Location	\$\$\$
Multimodal (Pedestrian, Bicycle, Transit, Vehicle)		
Mobile radar detecting speed feedback signs	Per Location	\$
Fixed radar detecting speed feedback signs	Per Location	\$
Automated speed safety cameras	Per Location	\$\$
Center medians or turn-restrictions	Per block	\$\$
Arterial driveway consolidation or removal	Per block	\$\$\$
Overhead street lighting	Per block	\$\$\$
Compact one-lane roundabouts	Per Location	\$\$\$
Full phase traffic signals: expense increases with number of lanes	Per Location	\$\$\$\$
Multimodal roundabouts with crossing refuges and bike ramps	Per Location	\$\$\$\$
Pedestrian-Bicycle grade-separated overpass over highway or railroad	Per Location	\$\$\$\$

Opportunity for Short-Term, Low-Cost, High-Impact Implementation in Burlington

Some streets in Burlington may be excellent candidates for low-cost, high-impact measures to implement sidewalks, bikeways, and crossing improvements on the Active transportation Network. One example of this is South Spruce Street (4,950 LF) from South Walnut Street/Fenske Lane to Washington Avenue.

South Spruce Street is a major north-south segment in the Active Transportation Network between Burlington Boulevard and the BNSF Railroad tracks and marked bicycle lanes are recommended. The street measures 40 feet-wide from curb to curb and many sections already have painted yellow curbs, alternating from side to side, to restrict on-street parking. Based on local observations, on-street parking utilization, where allowed, appears to be very low.

The City could develop a spreadsheet template and map depictions for the number of legal parking spaces (8' wide x 20' long) on each side of each block along South Spruce Street, as well as the sections where parking is restricted. A low-cost parking utilization study could be conducted over the course of several weeks, including morning, midday, and evening time periods on weekdays. If necessary, weekend parking counts could also be collected.

Parking utilization rates could be calculated for each block on each side of the street based on counts of occupied vs unoccupied parking stalls. If the suspected low parking utilization rates were to be verified, then the City could recommend on-street parking removal from both sides, which would simplify parking restrictions. The City could then rechannelize (restripe) South Spruce Street to install buffered bike lanes and strategically placed ADA-compliant crosswalks.



Figure 4.1. South Spruce Street in Burlington with Existing Parking Restrictions

Regional Examples: Before & After Implementation of Street Reconfiguration for Sidewalks & Bike Lanes

Several examples of low-cost, high-impact street reconfigurations to implement sidewalk and bicycle facilities are provided below from cities in Skagit and Whatcom counties.

Figure 4.2.

Orleans Street in Bellingham Parking Removed 1 Side to install Bike Lanes

September 2012



July 2019



Figure 4.3.

Woburn Street in Bellingham Parking Removed 1 Side to install Bike Lanes

September 2012



October 2021



Figure 4.4.

Eldridge Avenue in Bellingham Parking Removed Both Sides to install Buffered Bike Lanes

September 2018

May 2025



Figure 4.5.

Commercial Avenue (7th Street) in Anacortes Rechannalization to install Buffered Bike Lanes

September 2012



June 2019



Figure 4.6.

4th Street (E. Fulton St) in Mount Vernon Rechannelization to install Bike Lanes

August 2008



September 2024



Figure 4.7.

Rio Vista Avenue (Section Street) in Burlington Reconstruction to install Sidewalk + Bike Lanes

September 2012



September 2024



Figure 4.8.

Section Street (E Fairhaven Ave) in Burlington Reconstruction to install Sidewalk + Bike Lanes

September 2008



September 2024



Figure 4.9.

Ellis Street in Bellingham 5-to-3-lane Road Diet to install Bike Lanes

November 2015



May 2025



Table 4.2.

Table 4.3 - Grant Program Sources and Timelines to Implement Burlington ATS Plan																
PROGRAM TITLE	PREREQUISITES	FUNDING SOURCE	AWARD AGENCY	LOCAL MATCH	JAN	FEB	MAR	APR	MAY	JUN	JUL	AUG	SEP	OCT	NOV	DEC
Washington State Legislature		State	State		NOFO	APP				NOA						
Safe Routes to School (SRTS)	Comprehensive Plan & 6-Year TIP Project	Federal	WSDOT	None ²		NOFO			APP		NOA ⁴					REC ³
Pedestrian Bicycle Program (PBP)	Comprehensive Plan & 6-Year TIP Project	State	WSDOT	None ²		NOFO			APP		NOA ⁴					REC ³
Regional Mobility Grant	Transit Agency Transit Development Plan	State	WSDOT			Pre-App	PA Due	NOFO	NOA	APP						
Safe Streets and Roads for All (SS4A)	Comprehensive Safety Action Plan	Federal	USDOT	20%			NOFO			APP						NOA
Better Utilizing Investments to Leverage Development (BUILD) - Urban application		Federal	USDOT	0-20%												
Surface Transportation Block Grants (STBG)	Comprehensive Plan & 6-Year TIP Project	Federal	SCOG	13.5%				NOFO		APP		NOA				
Transportation Alternatives (TA)	Comprehensive Plan & 6-Year TIP Project	Federal	SCOG	13.5%				NOFO		APP		NOA				
Transportation Improvement Board (TIB)																
Urban Arterial Program (UAP)	Federal Functional Classified Street	State		10-20%						NOFO		APP			NOA	
Complete Streets Program (CSP)	Adopted Complete Streets Ordinance	State		10-20%						NOFO		APP			NOA	
Active Transportation Program (ATP)	Federal Functional Classified Street	State		10-20%						NOFO		APP			NOA	
Reconnecting Communities		Federal	USDOT				NOA					NOFO	APP			
Highway Safety Improvement Program (HSIP)	Local Road Safety Plan	Federal	WSDOT			APP							NOA	NOFO		
City Safety Program (Even Years)	"	"	"	N/A ¹		"							"	"		
County Safety Program (Odd Years)	"	"	"	N/A ¹		"							"	"		
Timeline Acronyms:																
NOFO = Notice of Funding Opportunity		Pre-App = Pre-Application		APP = Application Due				REC = Recommended to Legislature				NOA = Notification of Award				
Notes:																
1. No local match required as long as agency obligates funds according to schedule.																
2. No local match required to offer equitable access to grant funds for low-income agencies.																
3. WSDOT issues list of prioritized recommended projects based on preliminary funding allocation.																
4. Funding dependent on bi-ennial State legislative budget adopted July 1																

Annual Tracking, Monitoring, and Reporting

Burlington's MMLOS standards, multimodal performance measures, and active transportation and safety priorities are evaluated each year with staff findings and recommendations provided in an **Annual Concurrency Report** to document progress on completion of the citywide multimodal transportation system and Active Transportation Network. This information is used for transportation planning, investment, and partnership purposes rather than development review.

The annual evaluation of MMLOS standards, project improvement needs, and project cost estimates help to highlight needs in the Active Transportation Network and identify where significant gaps in the system need to be addressed to serve the land use plan, the safety plan, or other City priorities.

City staff and elected officials use the Annual Concurrency Report to help make informed investment decisions in the annual six-year transportation improvement program (TIP) process.

The Annual Concurrency Report helps City staff to bridge the gap between short-term needs and the long-term project list identified in the Transportation Element including:

- Pedestrian, bicycle, and multiuse trail projects needed to complete the citywide ATN.
- ADA upgrades in the public street right-of-way for curb ramps, crosswalks, sidewalks, and Skagit Transit bus stops.
- Multimodal safety improvements for people walking, biking, rolling, riding transit, and driving vehicles, such as crossings, streetlights, medians, or turn-restrictions.
- Intersection improvements, such as ADA upgrades, traffic calming, stop control, traffic signals, lead pedestrian interval phases, and roundabouts.
- Opportunities to remove barriers from safely crossing I-5, SR 20, or BNSF railroad tracks.

Chapter 5 - Interjurisdictional Coordination

Skagit County

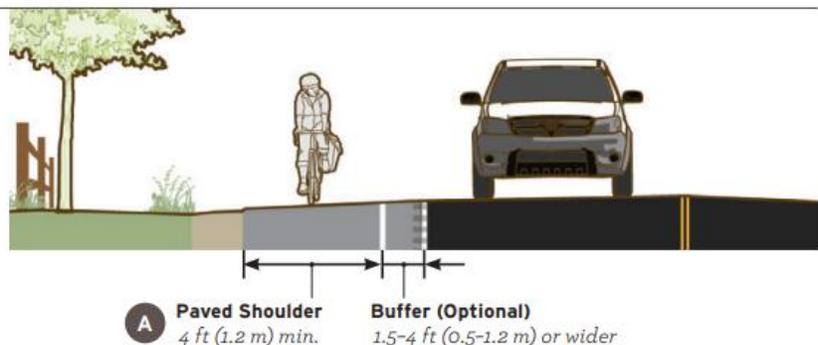
Burlington's UGA is under the jurisdiction of Skagit County, which has established a countywide Active Transportation Network (See Figure 5.2) with improvement needs for County roads and SR 20 listed in the [Transportation Element of the 2025 Skagit County Comprehensive Plan](#) (Adopted June 23, 2025). Skagit County receives assistance in active transportation planning in rural and urban growth areas from the SCOG Non-Motorized Advisory Committee (NMAC). Streets in the Burlington UGA that are recommended for pedestrian and bicycle improvements could be potential candidates for City-County funding partnerships to leverage state or federal grants.

In rural areas outside the Burlington UGA, Skagit County has established a minimum 4-foot-wide paved shoulder as the MMLOS standard for its Active Transportation Network, per FHWA guidance and as shown below. Some County roads and SR 20 currently have paved shoulders, and while this may not be ideal for comfort and safety, paved shoulders can serve as places to walk and bike outside of the vehicle travel lane. In a rural environment, a 4-foot paved shoulder is considered the minimum standard for a designated bicycle facility (Source: [FHWA Small Town and Rural Design Guide, 2017](#); Paved Shoulder illustration below).

Figure 5.1. Minimum Active Transportation LOS Standard

Paved Shoulder

Shoulders can improve bicyclist comfort and safety when traveling in higher speed and/or volume situations but only when adequate width is provided. If used, locate rumble strips on the edge line or within a buffer area that will not reduce usable space for bicyclists.



Source: [FHWA Small Town and Rural Design Guide, 2017](#)

City of Mount Vernon

The City of Mount Vernon, immediately to the south across the Skagit River and connected by Burlington Boulevard-Riverside Drive, has also completed an Active Transportation and Safety Plan that establishes an Active Transportation Network, MMLOS standards, and a prioritized list of pedestrian and bicycle improvements. Burlington and Mount Vernon can share information regarding City priorities for active transportation improvements to maximize integration, connectivity, and mutual benefits.

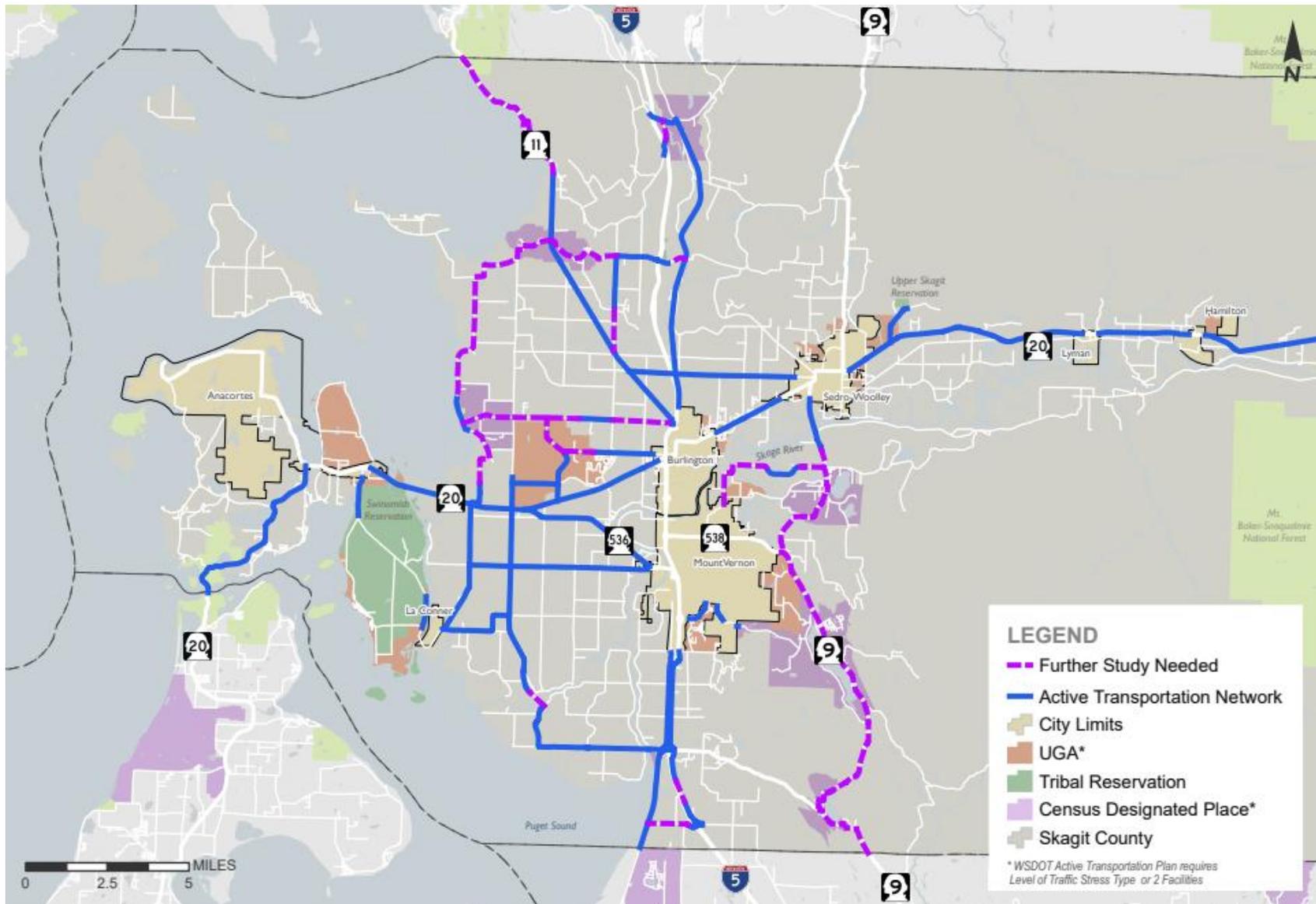


Figure 5.2. Skagit County Active Transportation Network - West

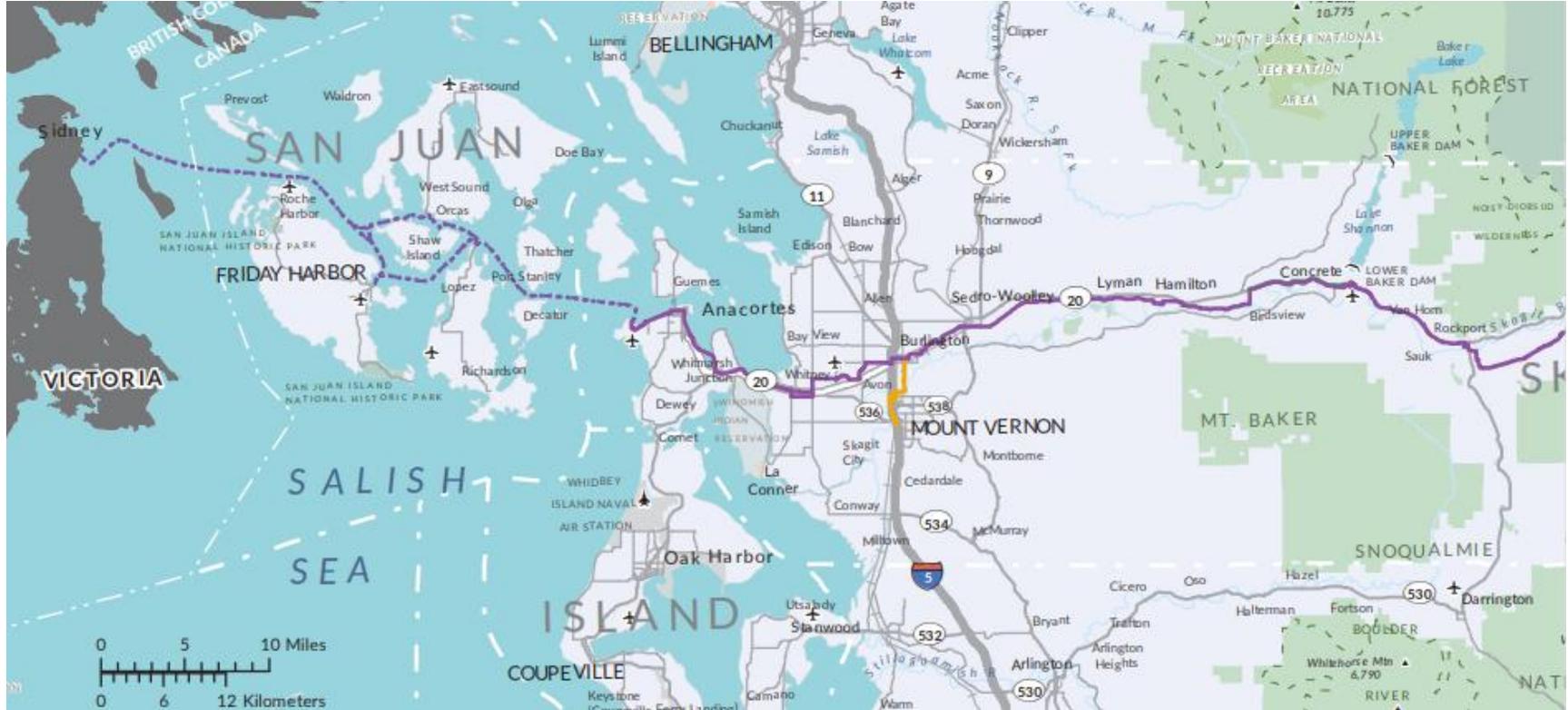


Figure 5.3. U.S. Bike Route 10 (USBR 10) through San Juan County via WSDOT Ferry and Skagit County, Washington*
**Note: USBR 10 generally follows SR 20, but many portions of this bike route do not provide dedicated bicycle facilities.*

Skagit Transit

The City of Burlington is not a public transit provider but is within the [Skagit Transit](#) Public Transportation Benefit Area (PTBA) funded by sales tax to provide public transit service. Skagit Transit currently provides fixed-route, complementary ADA paratransit, rideshare/vanpool services and operates and maintains the Chuckanut Park and Ride in north Burlington served by Routes 101, 208, 301, and 513. Skagit Transit currently provides the following services in Burlington:

Fixed-Route, Complementary ADA Paratransit, & Rideshare (Vanpool)

Skagit's fixed route services that serve Burlington include four routes, made up of:

- **Route 101** connects key locations in Burlington, providing regular transit service for both daily commuters and occasional riders. The route starts at the Chuckanut Park & Ride with stops at Dick's Sporting Goods and the Library for shoppers, students, and library patrons. On weekdays, Route 101 interlines with Route 513.
- **Route 208** starts from Skagit Station in Mount Vernon and offers easy access to essential daily services and commuter conveniences, such as Fred Meyer Grocery on Burlington Boulevard and ending at the Chuckanut Park & Ride in north Burlington. According to the 2024-2029 TDP, Route 208 has the highest ridership of any Skagit Transit route offered.
- **Route 301** connects Chuckanut Park & Ride in north Burlington to United General Hospital and the Sedro-Woolley Park-N-Ride, providing regular transit service for both daily commuters and occasional riders.
- **Route 513** operates Monday – Friday. For transportation on Saturday to March's Point or Anacortes riders must check the schedules for Routes 615 and 411W. There are no Skagit Transit or Island Transit buses operating between Mount Vernon and Anacortes on Sunday. Riders may contact Skagit Transit to request a route deviation to the Skagit Regional Airport.

The routes listed above provide 60-minute frequency and run between 5-7 days per week. ADA paratransit service is available within $\frac{3}{4}$ of a mile of fixed-route services and operates the same hours as fixed-route service. The Rideshare Program allows commuters traveling to common destinations to pay Skagit Transit a monthly fee and per mile charge to use a Skagit Transit-owned and maintained van to vanpool to work if the trip begins or ends in Skagit County.

Skagit Transit's [2024-2029 Transit Development Plan \(TDP\)](#) guides the development of public transportation in Burlington and Skagit County. For 2024-2029, the plan focuses on implementing service recommendations from several planning studies, expanding the vehicle fleet and facilities, and investing in new technologies.

Skagit Transit developed a [Long-Range Transit Plan](#) (LRTP - Adopted June 18, 2025) that establishes a strategic framework for enhancing and expanding transit services over the next decade. This comprehensive plan aims to improve the experience for current riders while attracting new users by offering more efficient, accessible, and sustainable transportation options. **Skagit Transit's LRTP includes recommendations for changes to transit routes serving Burlington and the City should continue to collaborate with Skagit Transit on future land use and active transportation needs, as well as opportunities for funding partnerships.**

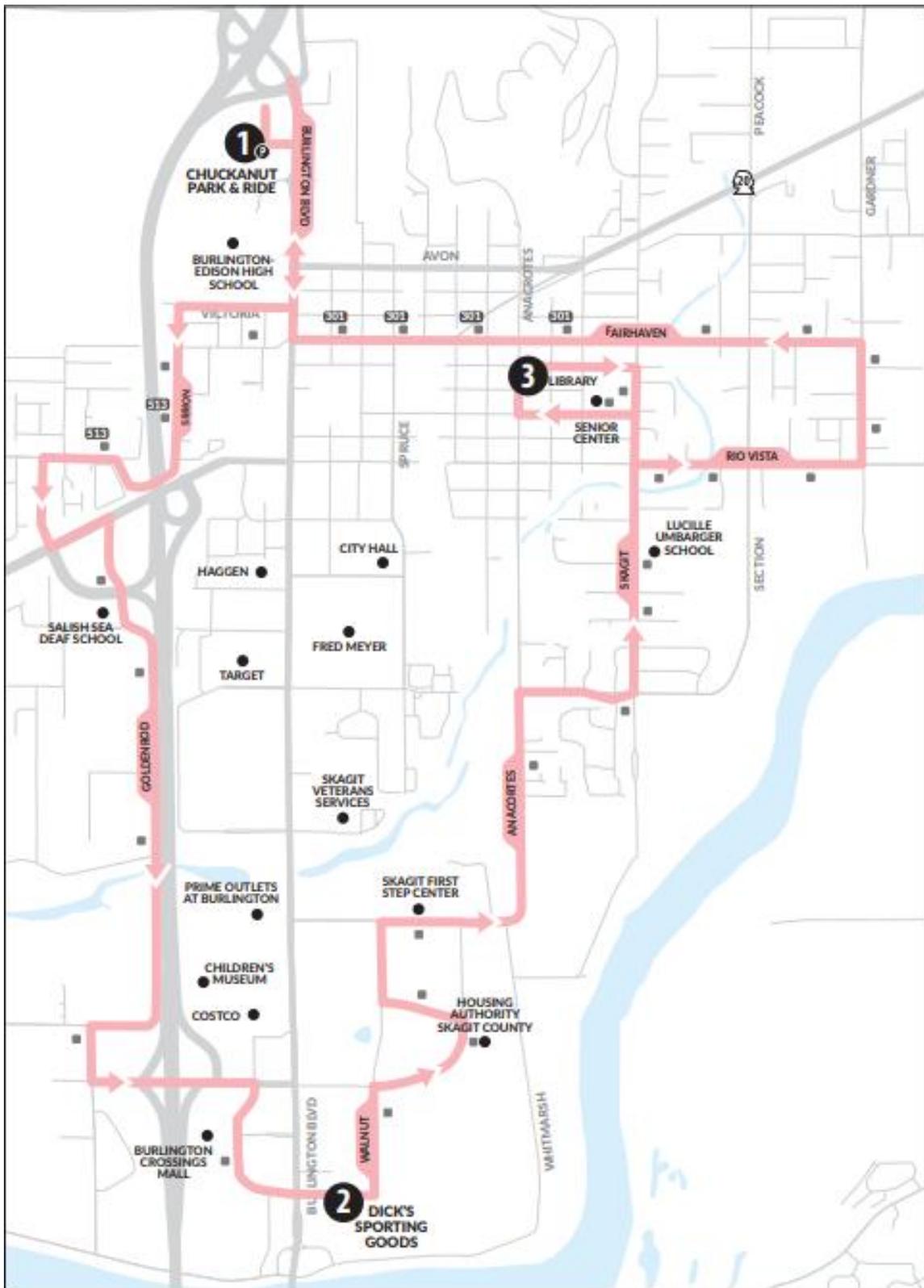


Figure 5.4. Skagit Transit Route 101

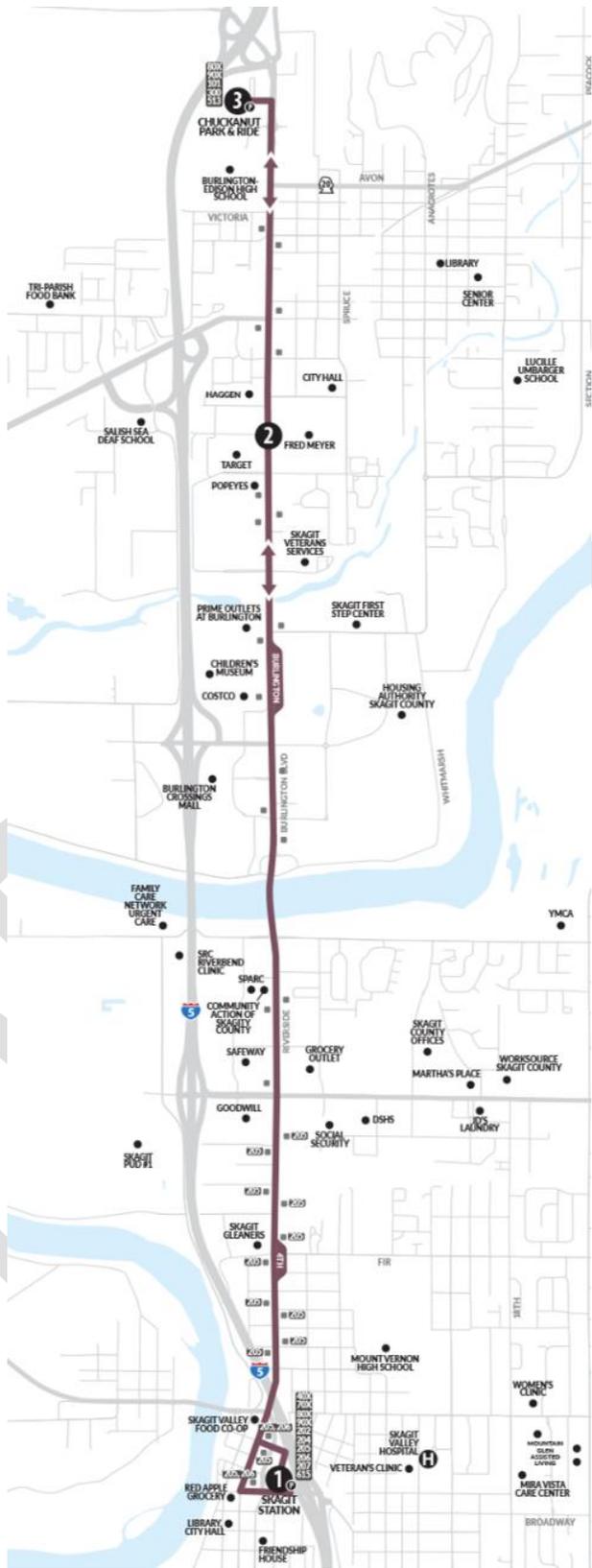


Figure 5.5. Skagit Transit Route 208

Figure 5.6. Figure 5.6 Skagit Transit Route 301





Figure 5.7. Skagit Transit Route 513 Burlington to Anacortes

DRAFT

Skagit Council of Governments

[Skagit Council of Governments \(SCOG\)](#) is the Regional Transportation Planning Organization (RTPO), as enabled by Washington state law, and the Metropolitan Planning Organization (MPO), as enabled by federal law. SCOG is also the administrator for the Growth Management Act (GMA) coordination process in Skagit County. SCOG works with State and Federal legislators to inform them about funding needs for transportation related projects and programs in Skagit County. SCOG serves as a forum for Skagit County jurisdictions to come together and form a regional voice. Issues with regional implications can be discussed and addressed through the SCOG. The SCOG Non-Motorized Advisory Committee (NMAC) advocates for sidewalks, bikeways, and trail facilities, consistent with the 2004 NMTP and has helped SCOG to update the [2024 Skagit County Bike Map](#) and the [2021 Skagit County Walking Trail Guide](#). ***It is important to clarify that a bicycle “route” may not include designated bicycle “facilities.”***

SCOG Regional Transportation Plan

The [Skagit 2045 Regional Transportation Plan](#) (Skagit 2045) is a multimodal long-range plan that establishes the strategic framework for meeting the Skagit region’s existing and future transportation needs. Developed through extensive coordination with affected jurisdictions and public input, Skagit 2045 provides a “toolbox” to facilitate cooperation and maximize resources to jointly select transportation projects and programs for regional funding and implementation through 2045. The existing plan is being updated to a [2050 Regional Transportation Plan](#).

SCOG Regional Safety Action Plan

SCOG is completing a [Regional Safety Action Plan \(RSAP\)](#) in 2025 with participation from the City of Burlington, Skagit County, and all other cities, towns, ports, tribes, and WSDOT. When completed, the RSAP will allow any participating agency to apply for USDOT Safe Streets and Roads for All (SS4A) implementation grants for multimodal transportation safety improvements, assuming the SS4A program continues to be funded by the federal government.

The RSAP, coupled with the Burlington Local Road Safety Plan (LRSP) that will be completed in late 2025, will allow the City to seek funding for multimodal and active transportation safety improvements for many of the projects recommended in this Active Transportation and Safety Plan for many years to come.

SCOG Rail Crossing Study

The [SCOG Rail Crossing Study](#) was completed in 2016 and evaluated the impacts to local roads from increased future train traffic at all 56 at-grade crossings in Skagit County. While the 2016 rail crossing study is somewhat dated now, the findings predicted that by 2040:

- North-South Mainline growth = 17 - 46 trains at crossings to between 55 - 85 trains
- Burlington-Anacortes Branch Line growth = 4 trains to 7 trains
- Burlington-Sumas Branch Line growth = 2 trains to 6 trains.

The SCOG study also found that with the predicted freight and passenger train volumes in 2040, daily gate-down time at at-grade crossings in Skagit County would increase by between 12 minutes and 1 hour 45 minutes. Associated impacts from increased future train traffic included safety impacts, delays for emergency response vehicles, increased vehicle queuing at at-grade street crossings, and delay for active transportation users.

The Rail Crossing Study identified 12 key crossings where impacts could be relatively high compared to other crossings in Skagit County, three of which are in Burlington in the following locations:

- Fairhaven Avenue near South Spruce Street
- SR 20 / Avon near South Spruce Street
- SR 20 / Burlington Blvd near East Orange Avenue

With or without trains present, at-grade street crossing of the north-south and east-west railroad tracks also act as a mobility barrier for people walking, biking, and rolling due to the varying road surface conditions, including open grooves and the angle of tracks across the road, as well as the presence of sidewalks or bikeways, and safe ADA-compliant surface crossing features. While grade-separated crossings may be the ideal form of mitigation, they are well-beyond the realistic funding capacity of the City of Burlington or WSDOT in the foreseeable future. Financially feasible options for improvement should include ADA-compliance for surface crossings and Intelligent Transportation System (ITS) information technology applications for approaching trains.

WSDOT

GMA requires local agencies to monitor vehicle LOS standards on state highways established by WSDOT and the SCOG to serve as a gauge for performance of the regional transportation system. WSDOT has not established LOS standards for pedestrian, bicycle, or transit on state highways, but Burlington has included SR 20 on its Active Transportation Network and will annually report on completeness of recommended sidewalks, bikeways, and ADA access at Skagit Transit bus stops.

As of August 1, 2025, and consistent with [RCW 47.04.035 Street access – Principles of Complete Streets – Requirements](#) when WSDOT conducts any work that costs \$1,000,000 or more on State highways abutting cities, UGAs, or census designated places, the active transportation facilities identified by local agencies must be incorporated into the WSDOT work. The [WSDOT Active Transportation Plan – 2020 and Beyond](#) calls for **Level of Traffic Stress (LTS) Type 1 or Type 2 active transportation facilities on State highways**, where feasible, to ensure that locally identified needs for connectivity and completeness are accommodated. LTS Type 1 and 2 facilities require separation from moving vehicle traffic either with a substantial physical barrier or by literally separating the facility from the roadway, such as a paved multiuse pathway. Adequate WSDOT public right-of-way must be available for either treatment and any environmental impacts, such as crossings of streams, wetlands, or buffers must be mitigated. The separated multiuse pathway that has been constructed along SR 20 between Burlington and Sedro-Woolley provides an excellent example of an LTS Type-1 facility for people walking, biking, and rolling. It should be noted that WSDOT does not currently have any funding dedicated to implement the RCW 47.04.035 Complete Streets initiative, nor any funded capital improvement projects on SR 20 within Burlington.

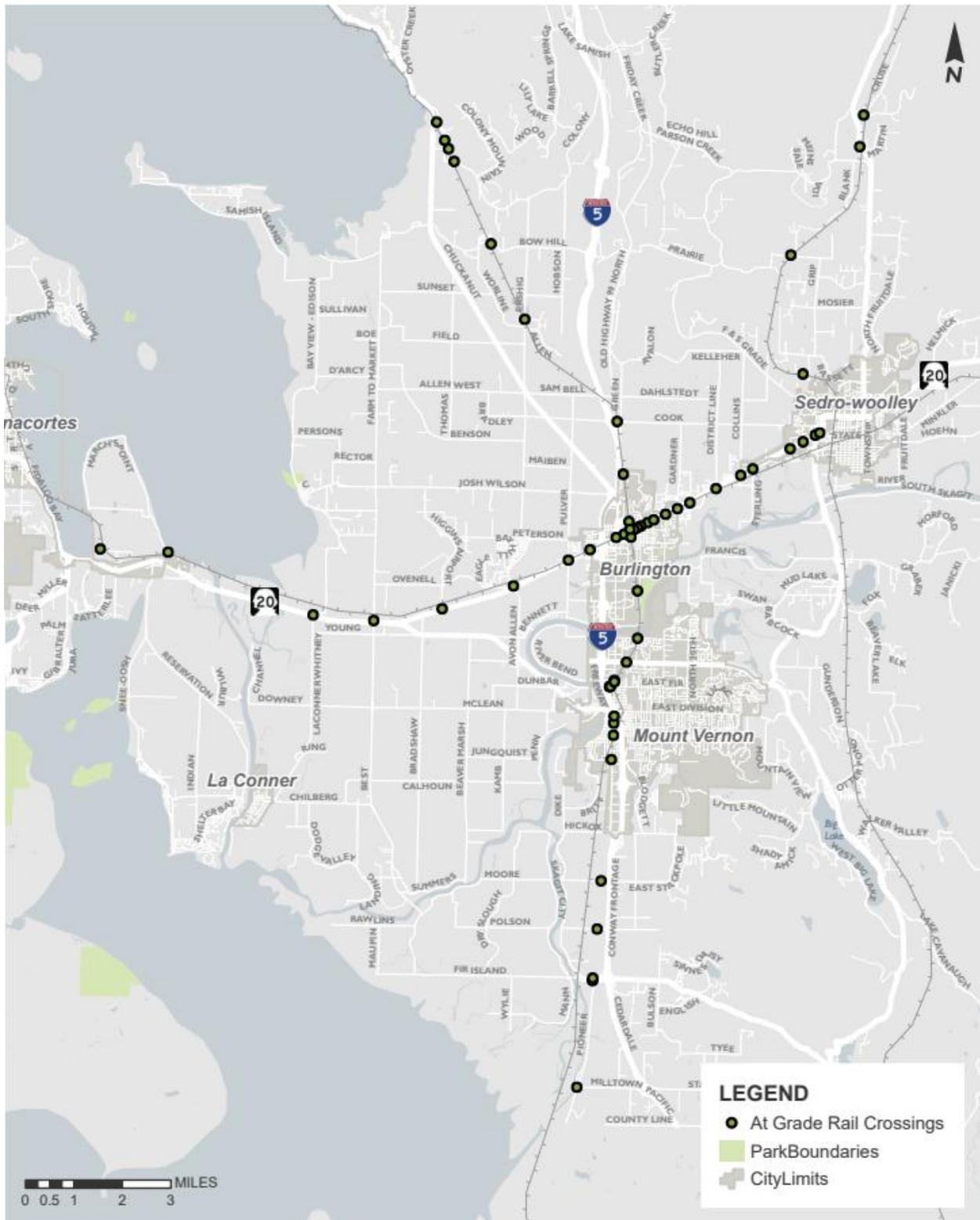


Figure 5.8. Skagit County At-Grade Railroad Crossings

APPENDICES

APPENDIX A – SAFETY ANALYSIS

APPENDIX B – SOCIAL DEMOGRAPHICS AND EQUITY ANALYSIS

APPENDIX C – VEHICLE LOS ANALYSIS

APPENDIX D – 2026 LOCAL ROAD SAFETY PLAN (Completion expected February 2026)

DRAFT

APPENDIX A – SAFETY ANALYSIS

DRAFT

Introduction

In May 2025, Transpo Group obtained crash data from the Washington State Department of Transportation (WSDOT) for the 5-year analysis period of 2020-2024 in Burlington, Washington.

Historical data was analyzed to reveal information such as the quantities of different crash types, the quantities of crashes resulting in fatal and serious injuries (FSI), and the quantities of different contributing circumstances, with particular focus on crashes affecting pedestrians and bicyclists. The results of the historical analysis provide a picture of crash trends over the five-year period, allowing for the identification of risk factors that may be addressed to improve the safety of the City's transportation system.

The crash data was then mapped and analyzed spatially to identify contributing circumstances at specific locations that displayed a high number of overrepresented crash types and severe crashes. The spatial analysis enabled the identification of facility characteristics that may be affecting crash frequency and severity such as wide crossings or missing pedestrian facilities.

Combining the historic and spatial analysis revealed the risk factors, showing what types of contributing behaviors and facility characteristics present the greatest risk to roadway users. This information is then used in the selection of future project components that can be implemented to reduce the risk of fatal and serious injury crashes. For example, an intersection that has a high number of crashes involving not yielding to pedestrians may be improved by the selection and implementation of a countermeasure that has been shown to increase driver yielding behavior, such as a leading pedestrian interval that allows pedestrians to cross one to three seconds prior to the signal for vehicle traffic turning to green.

Historical Crash Analysis

The following section summarizes the historical crash analysis in the City of Burlington from 2020 through 2024.

Between January 1, 2020, and December 31, 2024, there were a total of 1,332 crashes reported in the City of Burlington, including portions of SR 11 and SR 20 located within the city. Figure 1 displays the absolute number of crashes per year in the City and shows that while the past year saw fewer reported crashes than the previous, in general the number of crashes per year is rising slowly. Due to the unavailability of average daily traffic counts (ADT), crashes are not indexed to the rate of traffic, rather crash totals and percentages within respective crash categories (type, severity, and location) are compared to the totals of similar crashes reported in Western Washington cities with populations comparable to that of Burlington¹ where possible. Between January 1, 2019, and December 31, 2023, there were a total of 8,278 crashes in the Western Washington cities evaluated. Figure 2 shows that number of crashes per year in Western Washington cities with similar populations is declining.

¹ Cities included in the Western Washington cities analysis were Blaine, Du Pont, Fife, Newcastle, Port Townsend, Poulsbo, Snohomish, Stanwood, Sumner, and Yelm.

Figure 1. Crashes per Year 2019-2023 Burlington, WA

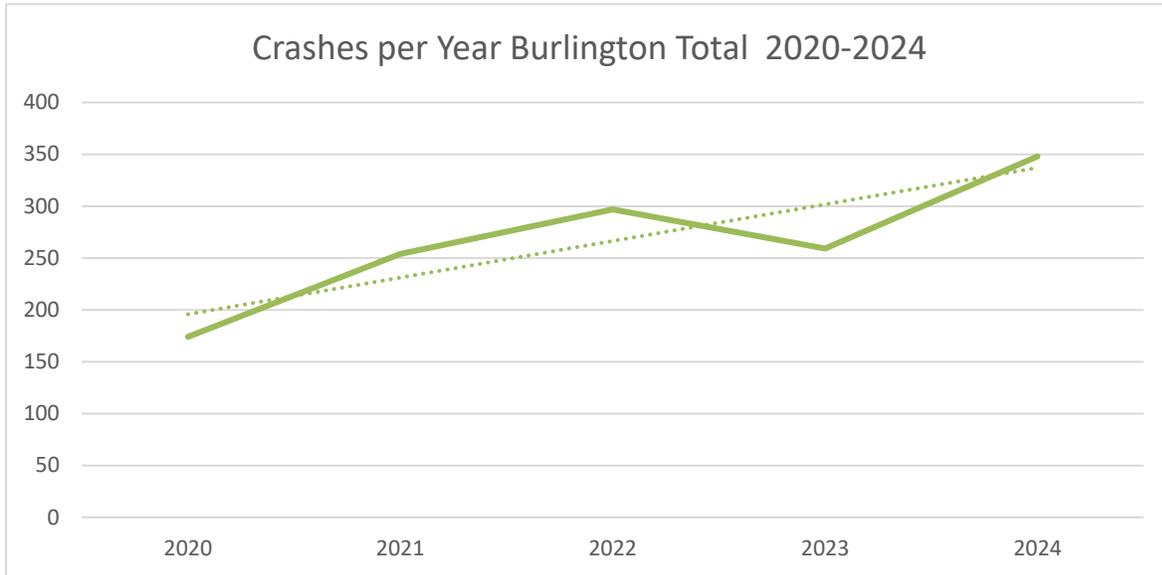
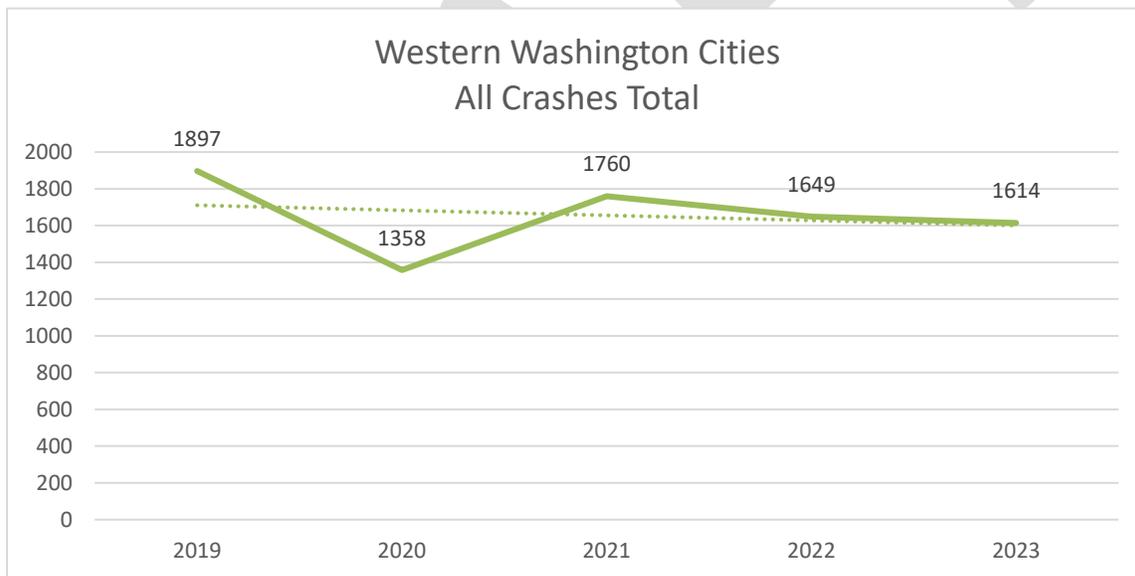


Figure 2. Crashes per Year 2019-2023 Western Washington Cities



Summary data for all crashes were reviewed to identify any overrepresented types of crashes and causes of crashes related to the transportation user (drivers, cyclists, and pedestrians) and the roadway environment (roadway geometry, characteristics, enhancements, etc.) on Burlington streets.

Crash Type

Of the 1,332 crashes reported in Burlington during the analysis period, the most often reported crash types of all severity were:

- 430 Rear end (32%)

Rear end crashes occur when one vehicle following another strikes the forward vehicle from behind.

- 314 Angle (24%)

Crashes occurring when one vehicle struck another at or near a 90-degree angle were coded as angle crashes.

- 163 Approach turn (12%)

Approach turn crashes occur when a vehicle traveling forward strikes a vehicle making a left or right turn, or when a turning vehicle strikes a vehicle intersecting the turning path.

- 130 Sideswipes (10%)

Sideswipe crashes occur when vehicles traveling in the same or opposite directions collide along the length of their sides.

In addition, there were 39 crashes (3%) that involved pedestrians or bicyclists. Table 1 lists Burlington crash totals in comparison with crash totals for Western Washington cities with similar population sizes and shows that percentages of angle, approach turn, and pedestrian/cyclist crashes are all higher than those of Western Washington cities. Figure 3 shows the percentages of each type of crash in the statistical distribution.

Table 1. Crash Totals for Burlington and Western Washington Cities (All Severity)

Crash Type	Burlington Crashes	% of Burlington Crashes	W Washington Crashes	% of W Washington Crashes
Rear End	430	32%	2,810	34%
Angle	314	24%	1,513	18%
Approach Turn	163	12%	641	8%
Sideswipe	130	10%	901	11%
Pedestrian Bicyclist	39	3%	177	2%
Fixed Object	129	10%	1,278	15%
Other ¹	117	9%	908	11%
Head On	10	1%	50	1%
Total	1,332	100%	8,278	100%

1. Other crashes include crashes that involved parked vehicles, overturned vehicles and vehicles that collided with a railroad vehicle

Contributing circumstances are listed by the percentage that each circumstance was cited in conjunction with the most frequently reported crash types. Individual crashes may have more than one contributing circumstance cited. The percentages listed below represent the proportions that each circumstance type was cited in conjunction with each crash type, rather than percentages of circumstances for individual crashes. For example, if an individual crash cites a driver who is both speeding and distracted and the crash is a fixed object crash, both speeding and distraction will

contribute to the overall percentage of those circumstances that were associated with all fixed object crashes.

Rear ends

- 51% of rear end crashes cited drivers following too close
- 21% of rear end crashes cited drivers that were distracted
- 7% of rear end crashes cited drivers that were speeding

Angle

- 39% of angle crashes involved failure to yield
- 14% of angle crashes involved disregard for traffic controls
- 13% of angle crashes involved an improper maneuver
- 11% of angle crashes involved distracted driving

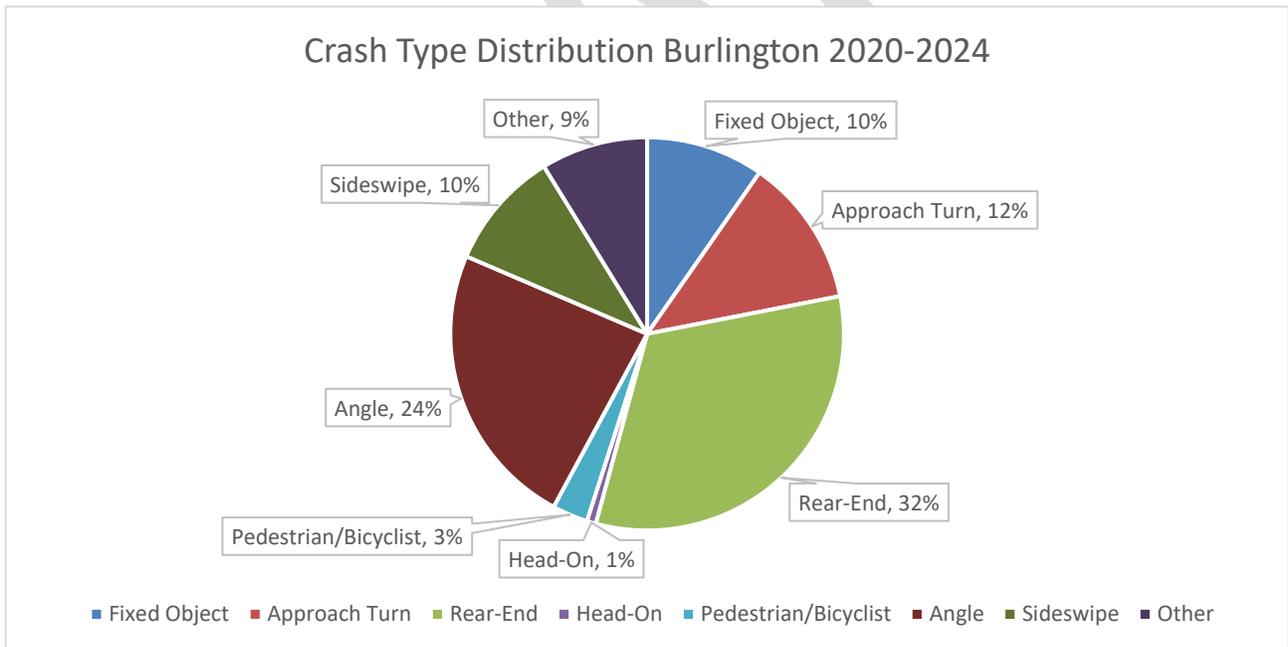
Approach turn

- 46% of approach turn crashes involved failure to yield
- 27% of approach turn crashes involved an improper maneuver
- 10% of approach turn crashes involved distracted driving

Sideswipe

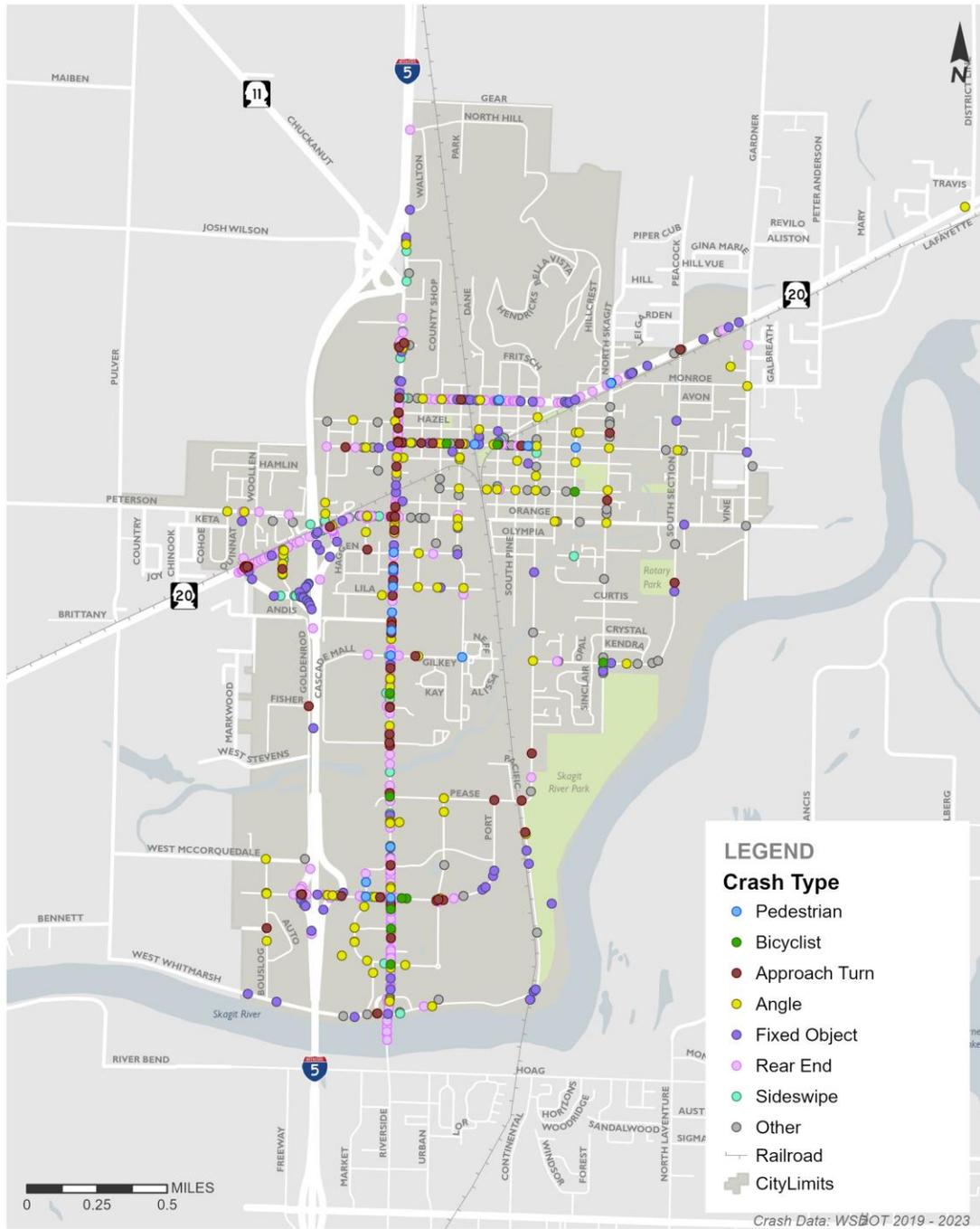
- 48% of sideswiped involved an improper maneuver
- 22% of sideswiped involved failure to yield
- 14% of sideswiped involved distraction

Figure 3. Burlington Crash Types



The geographic distribution of crashes by type is displayed on the map in Figure 4 and shows that crashes of all types are concentrated along Burlington Boulevard, portions of SR 20, Avon Avenue, Fairhaven Avenue, and George Hopper Road. A subset of crash types (angle, and “other”) are somewhat concentrated at stop-controlled intersections across the residential neighborhoods to the east of Burlington Boulevard and south of SR 20.

Figure 4. Geographic Crash Type Distribution



Crash Severity

All Crashes

Of the 1,332 crashes, 302 (23%) involved fatalities or injuries, as listed below.

- 3 (0.2%) resulted in a fatality
- 24 (1.8%) resulted in serious injury
- 257 (20.6%) resulted in minor or suspected injuries, and
- 1,030 (77.3%) resulted in property damage only.

FSI Crashes

Of the 27 crashes resulting in a fatality or serious injury (FSI), 10 (37%) were fixed object crashes, 6 (22%) involved a pedestrian or bicyclist, and 5 (19%) were “other” type crashes. The “other” type crashes typically involve parked vehicles but also include overturned vehicles and vehicles that collided with a railroad vehicle. Table 2 lists Burlington fatal and serious crash totals by type, compared to fatal and serious crash totals for Western Washington cities and shows that the percentage of FSI fixed object crashes, and “other” crashes were considerably higher than percentages of FSI fixed object and “other” crashes reported in Western Washington cities with similar populations. FSI pedestrian and cyclist crashes are overrepresented for both the City of Burlington and for Western Washington cities.

Table 2. Crash Totals for Burlington and Western Washington Cities (FSI)

Crash Type	Burlington Crashes	% of Burlington Crashes	W Washington Crashes	% of W Washington Crashes
Rear End	1	4%	17	10%
Angle	0	0%	28	17%
Approach Turn	3	11%	12	7%
Sideswipe	0	0%	7	4%
Pedestrian Bicyclist	6	22%	36	22%
Fixed Object	10	37%	30	19%
Other ¹	5	19%	15	9%
Head On	2	7%	17	10%
Total	24	100%	162	100%

1. Other crashes include crashes that involved parked vehicles, overturned vehicles and vehicles that collided with a railroad vehicle

Figure 5 and Figure 6 show that the number of fatal and serious injury crashes recorded each year in both Burlington and other Western Washington cities of similar populations are rising.

Figure 5. Burlington FSI Crashes 2020 - 2024

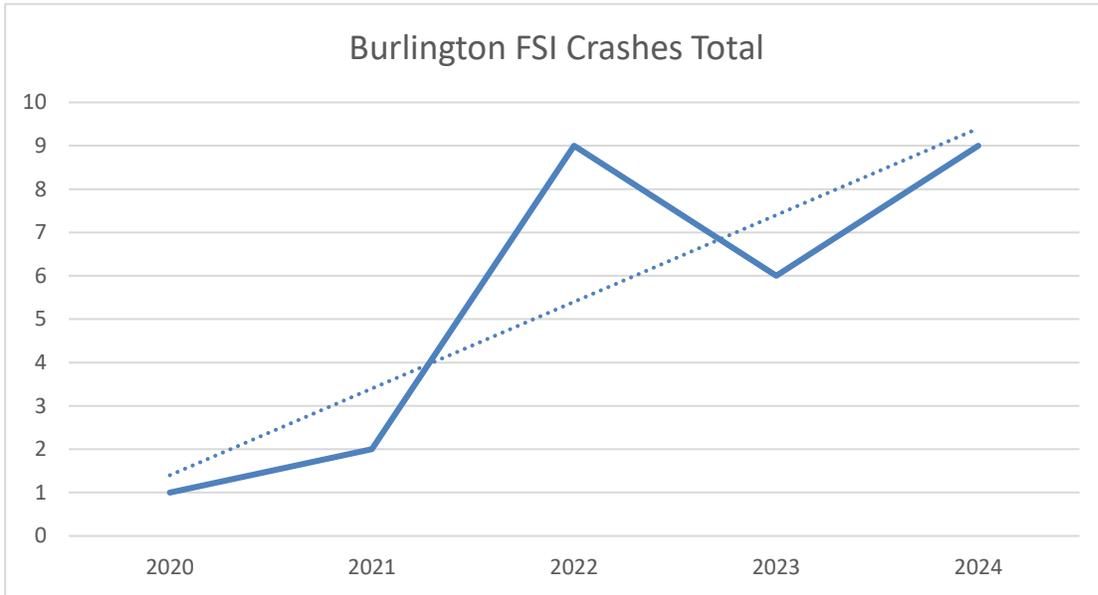
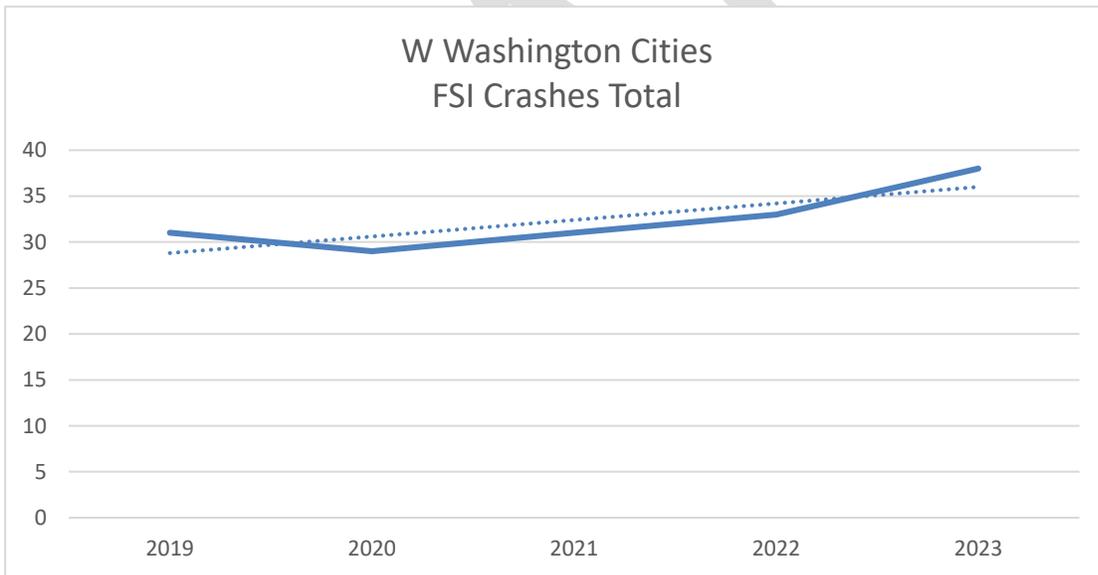


Figure 6. Western Washington FSI Crashes 2019 - 2023



Contributing circumstances are listed by the percentage that each was cited in conjunction with the most frequently reported fatal and serious injury crash types as follows:

FSI Fixed Object

- 56% speeding
- 44% intoxication
- 11% (each) distraction, asleep/emotional/ill, improper maneuver, disregard for traffic controls

FSI Other²

- 20% (each) speeding, intoxication, distraction
- 40% disregard for traffic controls

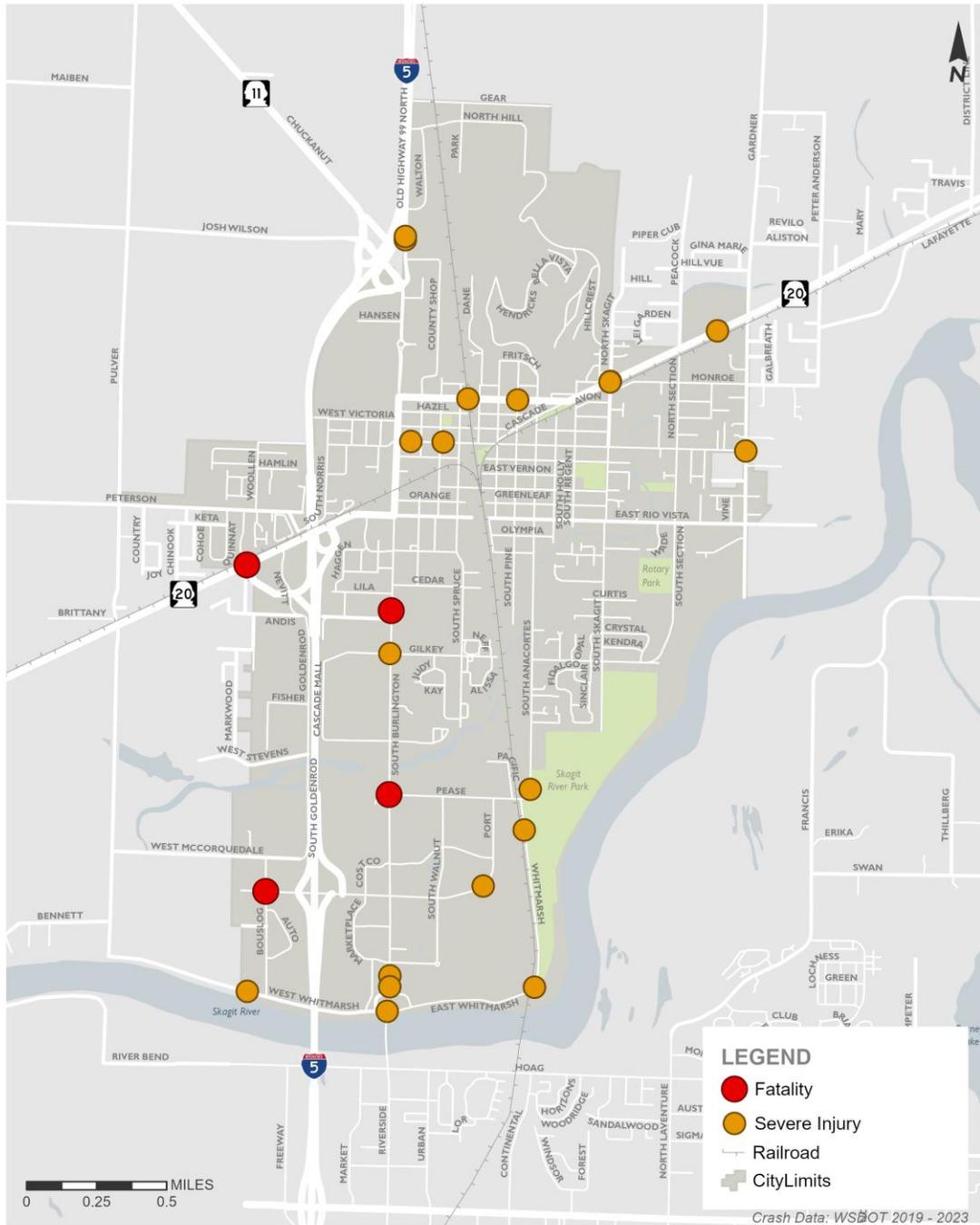
FSI Pedestrian/Bicyclist

- 20% (each) failure to yield (driver or active mode), distraction (driver or active mode), intoxication (driver or active mode)
-

Figure 7 shows the geographic spread of the fatal and serious injury crashes across Burlington. As seen in the figure, most of the fatal crashes occurred on higher volume roadways while the serious injury crashes are spread somewhat more evenly across the city.

² “Other” crashes include crashes that involved parked vehicles, overturned vehicles and vehicles that collided with a railroad vehicle.

Figure 7. Fatal and Serious Injury (FSI) Crashes



Fatal and Severe Injury Crashes
 Burlington Active Transportation & Safety Plan

FIGURE
03



M:\2411.24258.00 - Burlington Non-Motorized & Transportation Safety Plan\GIS\ArcGISPro\Transpo_Burlington_NonMotorized\Transpo_Burlington_NonMotorized.aprx

Crash Summary

Table 3 shows the 2020-2024 collision summary for major intersections throughout Burlington. Intersections in **red** show the locations where pedestrian or cyclist crashes occurred. Other intersections show an elevated proportion of crashes compared to other locations and should be considered for systemic risk mitigation. Systemic risk mitigation would mean applying certain treatments broadly to the transportation network to proactively reduce risk at locations which have similar characteristics to locations where collision history did show a disproportionate number of crashes. Locations where only a single crash occurred during the analysis period are listed because the crash involved a pedestrian or cyclist.

Table 3. Five-Year Collision Summary – 2020 to 2024

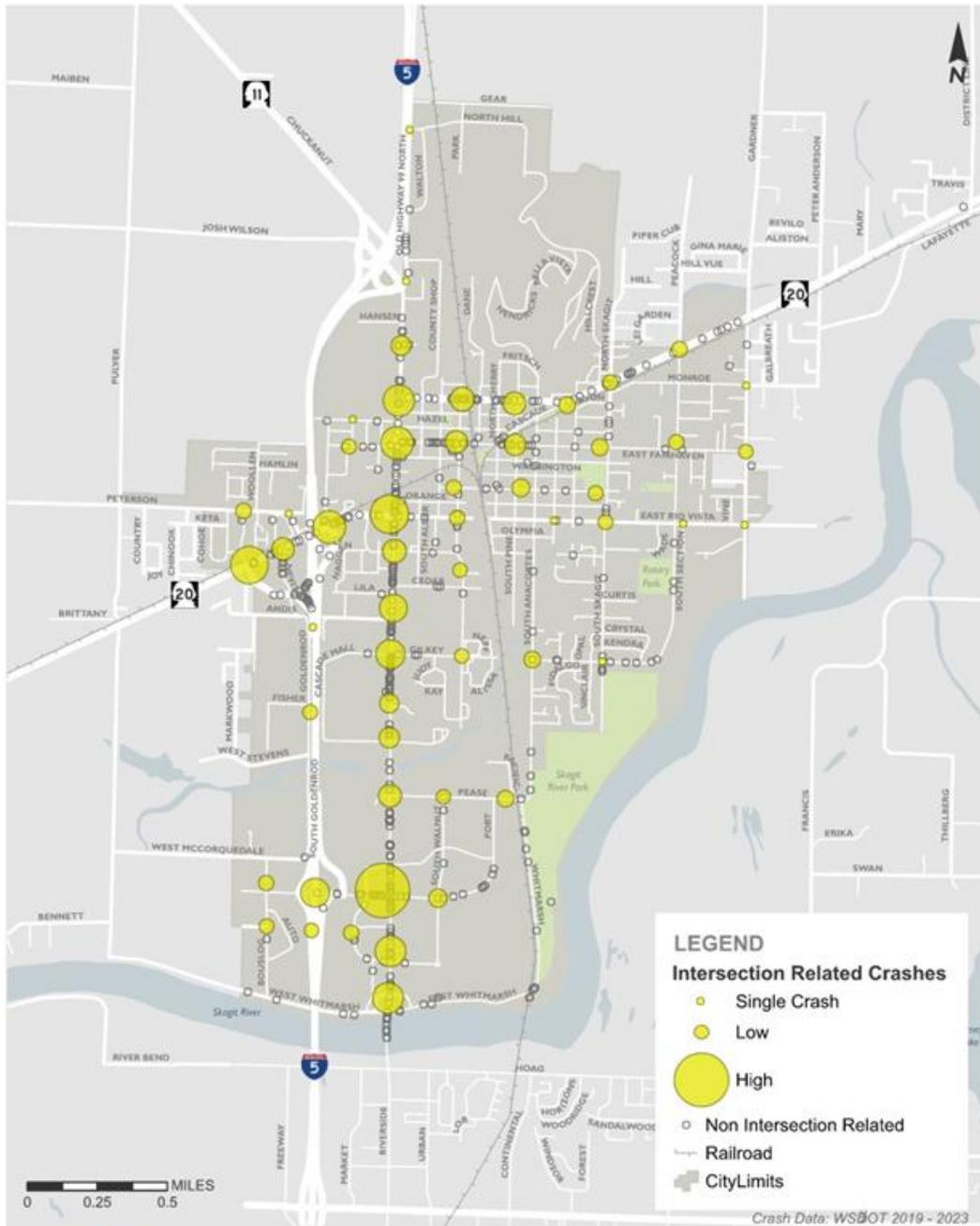
Location	Number of Collisions					Total	Annual Average
	2020	2021	2022	2023	2024		
Anacortes St/Rio Vista Ave	0	0	0	2	0	2	0.40
George Hopper/S Walnut	2	3	4	3	3	15	3.00
Skagit St/Fairhaven Ave	1	2	1	2	1	7	1.40
Spruce St/ Greenleaf Ave	0	2	0	1	3	6	1.20
Spruce St/Rio Vista Ave	2	0	1	4	1	8	1.60
SR20/Avon Ave	5	9	7	10	13	44	8.80
SR 20/Cherry St	4	2	4	2	2	14	2.80
SR 20/N Regent St	0	0	2	1	1	4	0.80
SR 20/Section St	0	0	2	2	1	5	1.00
SR 20/Skagit St	3	1	1	1	1	7	1.40
SR 20/Spruce St	1	4	6	0	2	13	2.60
Whitmarsh Rd/Pease Rd	0	2	1	3	1	7	1.40
Fairhaven Ave/Cherry St	2	0	2	1	0	5	1.00
Fairhaven Ave/N Oak St	1	0	1	2	1	5	1.00
Fairhaven Ave/N Regent St	0	1	0	0	0	1	0.20
Fairhaven Ave/S Walnut St	1	1	5	0	2	9	1.80
Fairhaven Ave/Alley w/o S Anacortes St	1	0	0	1	0	2	0.40
Greenleaf Ave/S Regent St	0	1	0	0	0	1	0.20
George Hopper Rd/Costco Dr	5	6	11	5	15	42	8.40
Skagit St/Gilkey Rd	1	0	3	0	3	7	1.40
Spruce St/Gilkey Rd	1	0	1	0	0	2	0.40
S Burlington Blvd /Andis Rd	5	7	10	2	2	26	5.20
S Burlington Blvd/Cascade Mall Dr	7	8	10	11	5	41	8.20
S Burlington Blvd/Costco Dr	1	5	4	4	11	25	5.00
S Burlington Blvd/Gilkey Rd	8	5	5	1	2	21	4.20
S Burlington Blvd/Marketplace Dr	3	9	8	10	11	41	8.20
S Burlington Blvd/Cedar St	2	4	3	5	3	17	3.40
S Burlington/E Sharon Ave (Haggen Driveway)	1	2	5	3	2	13	2.60
S Burlington Blvd/Sakuma Bros Ave	2	5	2	4	3	16	3.20
Burlington Blvd/ George Hopper Rd	12	11	30	14	16	83	16.60
Burlington Blvd / Pease Rd	2	4	11	10	11	38	7.60

Source: WSDOT July 2024

Figure 8 shows the crash frequency at intersections within Burlington. The intersection of George Hopper Road and Burlington Boulevard shows the highest proportion of intersection crashes, followed by SR 20 at Avon Avenue and George Hopper Road at Costco Drive. Several other

DRAFT

intersections along Burlington Boulevard and SR 20 also show frequent crashes.



Intersection Related Crashes
 Burlington Non Motorized and Safety Plan **FIGURE XX**
 DRAFT transpogroup

M:\2412425\30 - Subregion Non-Motorized & Transportation Safety Plan\GIS\ArcGISPro\Transpo_Burlington_NonMotorizedTranspo_Burlington_NonMotorized.aprx

Figure 8. Intersection Related Crashes

Pedestrian and Bike Crashes

Involvement of pedestrians or cyclists were reported in 34 (3%) of the 1,332 crashes, with 6 of the 34 ped/bike crashes (22%) resulting in a serious injury or fatality. Minor injuries were reported in 31 (79%) of the 34 ped/bike crashes. Property damage only (PDO) was reported in only 2 (5%) of the ped/bike crashes. Figure 9 shows the proportions of each crash severity in relation to others.

Analysis of reported pedestrian and bike crashes showed that while 2023 crash rates were generally equal to 2019 rates, the interim years, and in particular 2022 with 16 reported crashes, logged generally more crashes per year than previous, resulting in an overall escalating crash rate for crashes involving active mode users. The chart displayed in Figure 10 shows that the rate of reported crashes involving pedestrians and cyclists has more than doubled since 2019.

Figure 9. Pedestrian and Cyclist Crash Severity

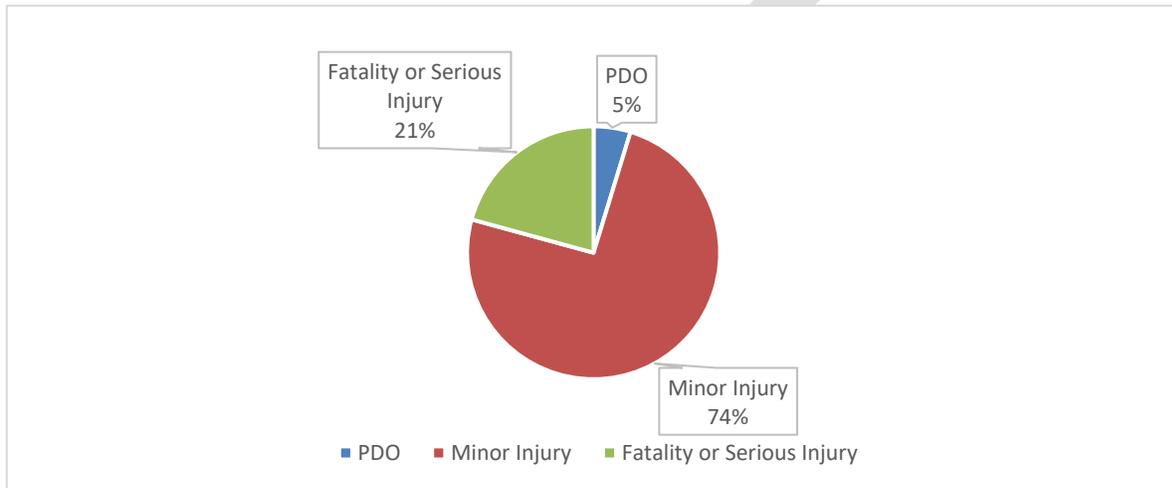
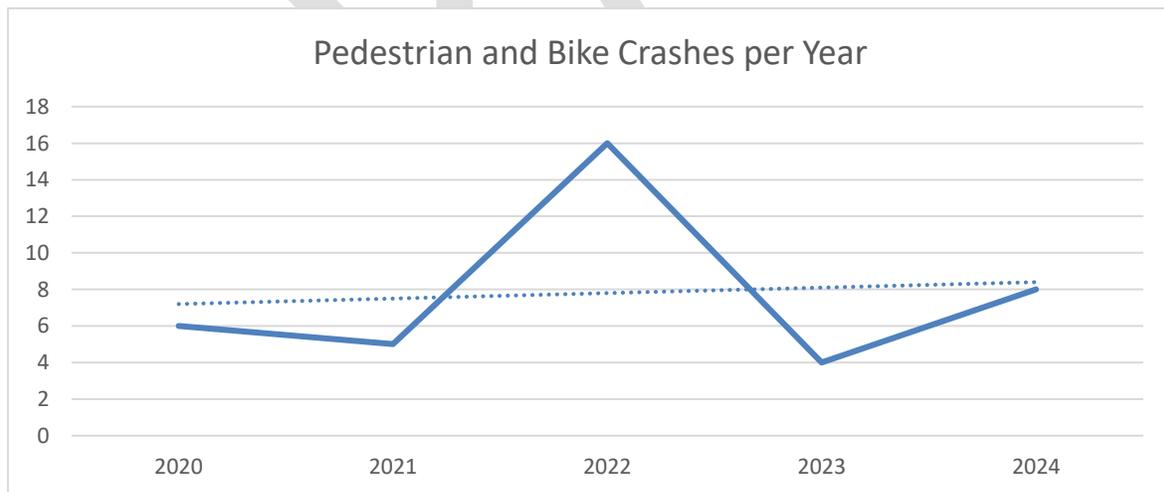
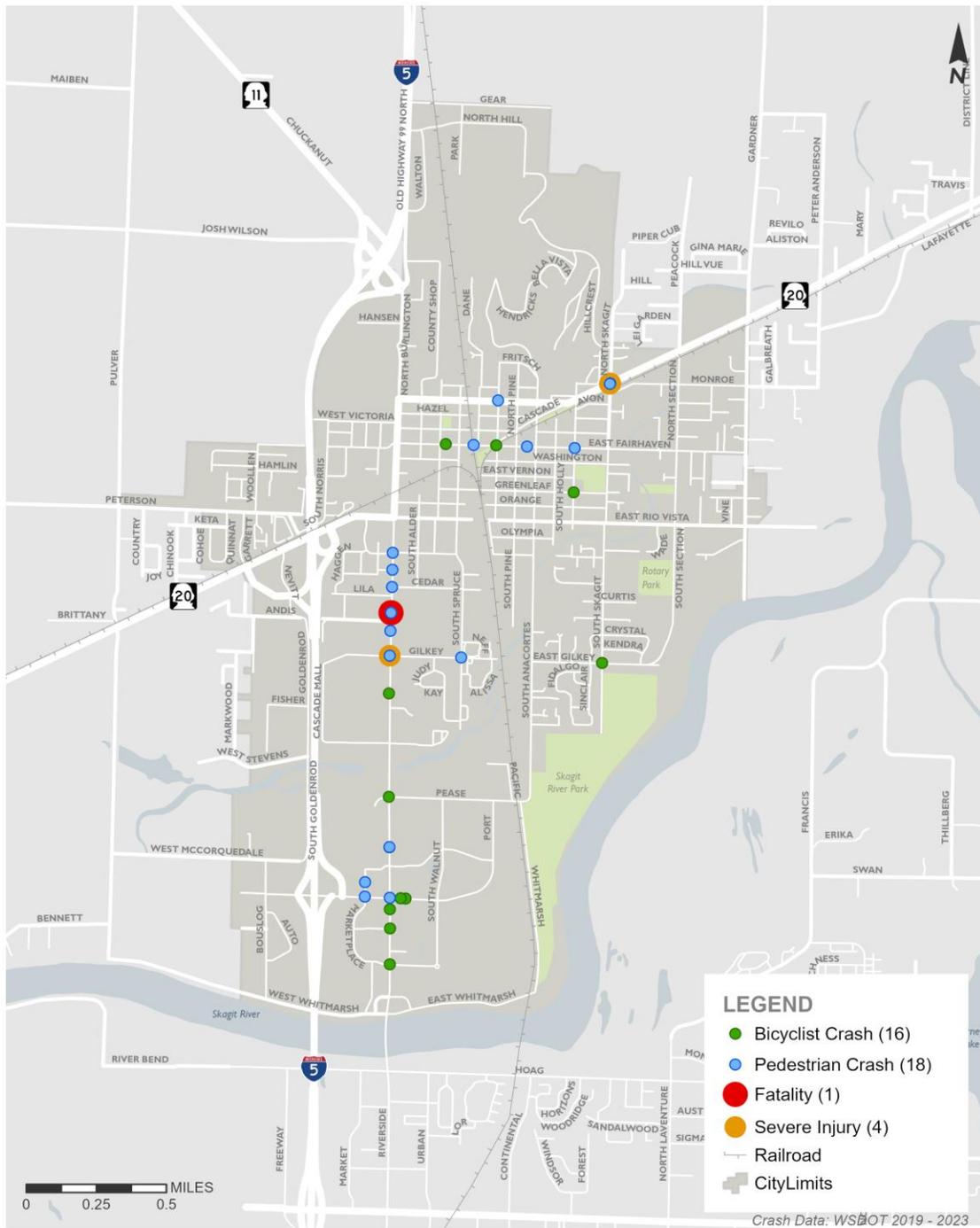


Figure 10. Pedestrian and Cyclist Crashes per Year



The geographic distribution of pedestrian and bike crashes is shown in Figure 11. As seen in the figure, crashes involving pedestrians and bicyclists are concentrated along Burlington Boulevard and Fairhaven Avenue. Fatal and serious injury active mode crashes were reported on Burlington Boulevard, and SR 20.

Figure 11. Pedestrian and Bicyclist Crashes



Pedestrian and Bicyclist Crashes
 Burlington Active Transportation & Safety Plan



FIGURE
07

M:\2411 24258 00 - Burlington Non-Motorized & Transportation Safety Plan\GIS\ArcGISPro\Transpo_Burlington_NonMotorized\Transpo_Burlington_NonMotorized.aprx

Intersection Analysis of Pedestrian and Bicycle Crashes

Locations where pedestrian and bike crashes occurred were reviewed for frequency, severity and contributing circumstances in pedestrian and bike crashes. The locations where pedestrian and bike crashes were reported and the number of crashes at each location, including the proportion of crashes at each location that resulted in fatality or serious injury, are shown in Table 4, below.

Locations that are overrepresented in terms of frequency or severity are highlighted in **red**. Table 2 shows that 100% of the reported pedestrian and cyclist crashes at SR 20 and Skagit Street, and 67% of the reported pedestrian and cyclist crashes at the intersections of S Burlington Boulevard and Gilkey Road, and S Burlington Boulevard and Marketplace Drive resulted in serious injuries or fatalities. In addition, 1 of the 2 pedestrian and cyclist crashes reported at S Burlington Boulevard and Andis Road resulted in a fatality. Intersections with the highest proportion of pedestrian and cyclist crashes were S Burlington Boulevard and George Hopper Road (18% of ped/bike crashes), and S Burlington Boulevard at Costco Drive, Marketplace Drive, and Gilkey Road (8% of ped/bike crashes, each). While there were no reported serious injury or fatal pedestrian or cyclist crashes at S Burlington Boulevard and George Hopper Road, the high proportion of crashes at the location significantly raises the risk of a severe outcome in future crashes and the location should be included in systemic risk reduction treatments.

Table 4. Pedestrian and Cyclist Intersection Crashes

Intersection Location	Pedestrian and Cyclist Crashes	% of Pedestrian and Cyclist Crashes	FSI Pedestrian and Cyclist Crashes	FSI % of Pedestrian and Cyclist Crashes
SR 20/Cherry Ave	1	3%	0	0%
SR 20/Skagit St	2	5%	2	100%
Fairhaven Ave/Cherry St	1	3%	0	0%
Greenleaf Ave/S Regent St	1	3%	0	0%
George Hopper Rd/Costco Dr	1	3%	0	0%
Skagit St/Gilkey Rd	1	3%	0	0%
Spruce St/Gilkey Rd	1	3%	0	0%
S Burlington Blvd/Andis Rd	2	5%	1	50%
S Burlington Blvd/Cascade Mall Dr	2	5%	0	0%
S Burlington Blvd/Costco Dr	3	8%	1	33%
S Burlington Blvd/Gilkey Rd	3	8%	2	67%
S Burlington Blvd/Marketplace Dr	3	8%	2	67%
S Burlington Blvd/Cedar St	2	5%	0	0%
S Burlington Blvd/Haggen Dr	2	5%	0	0%
S Burlington Blvd/Sakuma Bros Ave	1	3%	0	0%
S Burlington Blvd/George Hopper Rd	7	18%	0	0%
S Burlington Blvd/Pease Rd	2	5%	0	0%
Fairhaven Ave/Oak St	1	3%	0	0%
Fairhaven Ave/Regent St	1	3%	0	0%
Fairhaven Ave/Walnut St	1	3%	0	0%
Fairhaven Ave/Alley w/o S Anacortes St	1	3%	0	0%

Contributing Circumstances

Intersections where 2 or more pedestrian/bike crashes occurred were further analyzed to identify the most frequently contributing factors in crashes involving active mode users, listed in Table 5, along with facility characteristics present at the locations which contribute to challenging and risky conditions. Distraction, intoxication, and not yielding at locations characterized by high traffic volumes and wide right of ways with missing crossings adjacent to pedestrian destinations presented common themes.

Table 5. Contributing Circumstances in Pedestrian and Cyclist crashes

Location	Driver Contributing Circumstance	Pedestrian and Cyclist Contributing Circumstance	Facility Characteristics
S Burlington Blvd/George Hopper Rd	Intoxication (29%) Fail to Yield (14%) Distraction (14%)	Distraction (29%) Intoxicated (14%) Cyclist Speeding (14%)	Wide Crossings Unconsolidated Access Multiple Turn Lanes
S Burlington Blvd/Pease Rd	Fail to Yield (50%) Intoxication (50%) Distraction (50%)	None Listed	Sightline Obstructions Wide Crossings High Volumes
S Burlington Blvd/Cedar St	Fail to Yield (50%) Intoxication (50%)	Distraction (100%)	Lack of E/W Marked Crossing Wide Crossing Distances Transit Access High Traffic Volumes
SR 20/Skagit St	Distraction (50%)	Intoxication (50%) Fail to Yield (50%)	Railroad Trail Access Lack of N/S Marked Crossing Walkable Neighborhoods N/S of SR 20
S Burlington Blvd/Andis Rd	None Listed	Intoxicated (50%) Fail to Yield (50%)	High Density Residential Transit Access Grocery Access Lack of E/W Crossings High Traffic Volumes
S Burlington Blvd/Gilkey Rd	Fail to Yield (33%) Intoxicated (33%) Distraction (33%)	Failure to Yield (33%)	Wide Crossings Unrestricted Right Turns High Traffic Volumes Transit Access Proximity

Risk Factors

There were three primary themes that emerged from analysis of intersections where FSI crashes involving active mode users had occurred which comprise the primary risk factors for Burlington streets and intersections. The risk factors are:

- Arterial crossings that are wide, unmarked, and unimproved.
 - Especially problematic in locations that are in close proximity to high density housing, grocery shopping, transit access, neighborhood recreation.
- Driver and active mode users alike failing to yield in visually complex, high traffic volume environments with proximity to transit access.
 - Exacerbated by higher speeds on arterials and state routes, lack of driver expectation of active mode users on SR 20, fewer crossing opportunities/ poor gap tolerances.
- Driver and active mode user intoxication.
 - Exacerbated by lack of improved crossings/ environments that foster poor gap tolerance, high cognitive load and visually complex environment.

In addition to the issues listed above, the following risk factor presented for the most frequently reported crashes of all severity.

- Speeding and following too closely.
 - Exacerbated by high traffic volumes and congestion, stop and go traffic.

Preliminary Countermeasure Selection

The United States Department of Transportation Federal Highway Administration (USDOT FHWA) publishes a collection of safety countermeasures known as the Crash Modification Factor (CMF) clearinghouse³. Crash modification factors describe a multiplicative factor that indicates the proportion of crashes that would be expected after implementing a given countermeasure. A countermeasure with a given CMF of 0.85, for example, would be expected to reduce the relevant crash type by 15%.

Several of the most effective, case tested countermeasures are also published in the FHWA's selection of Proven Safety Countermeasures. The countermeasures recommended to address the specific risks on Burlington transportation facilities were selected from the list of Proven Safety Countermeasures based on several considerations including crash type, contributing user behaviors and location characteristics, cost and complexity of implementation, and expected effectiveness in addressing the risks.

Both the CMF Clearinghouse and the FHWA's Proven Safety Countermeasures should be understood as a toolbox of potential options for addressing various situations. These resources contain countermeasures as simple as adding high contrast lane striping, and as complex as installing a roundabout. Just as it would be unwise to select a screwdriver to pound in a nail, countermeasures selected to address crashes on Burlington streets were selected from the

³ <https://highways.dot.gov/safety/proven-safety-countermeasures>

toolbox of available options based on their appropriateness and applicability to the particular risk factors presented by the unique combination of characteristics present at each location, and the history of contributing circumstances that have been recorded in previous crashes. Other considerations when selecting recommended countermeasures included cost and complexity of implementation, effectiveness over the lifetime of the countermeasure, and likelihood of positive reception from roadway users.

Table 6, below, presents preliminary recommendations for countermeasures at intersections where there have been 2 or more crashes in the past five years that involved active mode users. Table 7 presents preliminary recommendations for systemic countermeasures to address the most frequently reported crashes leading to fatal and serious injuries (fixed object, and “other”), and the most frequently cited crash types of all severity (angle, and rear end).

Table 6. Preliminary Countermeasures for FSI Pedestrian and Cyclist Crashes

Location	Countermeasure Recommendations (CMF)
S Burlington Blvd/George Hopper Rd	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ Add marked e/w crossing on north leg of intersection • Leading Pedestrian Interval (LPI) signal phase • Targeted Enforcement (intoxication, speeding)
S Burlington Blvd/Pease Rd	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ Addition of “Watch for Pedestrian” pavement marking on right slip lane approach on Fashion Way • Leading Pedestrian Interval (LPI) signal phase • Targeted Enforcement (intoxication, speeding)
S Burlington Blvd/Cedar St	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ Add e/w midblock crossing with Pedestrian Hybrid Beacon (PHB) at Cedar St • Targeted Enforcement (intoxication, speeding)
SR 20/Skagit St	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ Add marked n/s crossing with Pedestrian Hybrid Beacon (PHB) ○ High-visibility crosswalk markings, lighting, and signing and pavement markings • Targeted Enforcement (intoxication, speeding)
S Burlington Blvd/Andis Rd	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ Add marked e/w crossing with Pedestrian Hybrid Beacon (PHB) ○ High-visibility crosswalk markings, lighting, and signing and pavement markings • Targeted Enforcement (intoxication)
S Burlington Blvd/Gilkey Rd	<ul style="list-style-type: none"> • Crosswalk Visibility Enhancements <ul style="list-style-type: none"> ○ High-visibility crosswalk markings, lighting, and signing and pavement markings • Leading Pedestrian Interval (LPI) signal phase • Targeted Enforcement (intoxication)

Table 7. Preliminary Countermeasures for Frequently Reported Crashes

Crash Type	Countermeasure Recommendations (CMF)
Fixed Object ¹	<ul style="list-style-type: none"> • Targeted Enforcement (cell phone use/distraction, speeding) • Widened Edge Lines
“Other” ²	<ul style="list-style-type: none"> • Road Diet <ul style="list-style-type: none"> ○ Angled parking on Fairhaven Ave reconfigured to parallel spots and bike lane • Targeted Enforcement on E Whitmarsh and Pease Rd/S Anacortes St (intoxication, speeding)
Angle ³	<ul style="list-style-type: none"> • Systemic Application of multiple low-cost countermeasures at stop-controlled intersections across the residential neighborhoods to the east of Burlington Boulevard and south of SR 20 • Road Safety Audit of Burlington Boulevard to identify preferred treatments at driveways and stop controlled side streets
Rear End ⁴	<ul style="list-style-type: none"> • Targeted Enforcement (intoxication, speeding) • High Friction Surface Treatment at crosswalk approaches

1. Fixed object was the most frequently reported FSI crash type.
2. “Other” crashes included collisions with parked vehicles, railroad vehicles, and overturned vehicles, and were the second most frequently reported FSI crash type.
3. Angle crashes were most frequently reported of all severity.
4. Rear end crashes second most frequently reported of all severity.

Prioritization of Countermeasures

In support of the development of the Burlington Active Transportation & Safety Plan, an equity analysis has been conducted to identify communities experiencing socioeconomic disadvantages such as underinvestment in transportation facilities that may disproportionately elevate risks. The results of the equity analysis can be a powerful tool to assist the City when prioritizing the implementation of safety countermeasures as they show where need is greatest. Additional priority factors, such as land use, connectivity benefit, community destinations, level of traffic stress, and individual project cost and complexity can help the City make decisions on active transportation and safety investments.

Conclusion

Fixed object crashes represented 37% the fatal and serious injury crashes in Burlington, followed by pedestrian/cyclist crashes at 22%, and “other” type crashes at 19%.

Systemic and spatial analysis were used to identify risk factors and locations where high numbers of overrepresented crash types had been recorded. Risk factors included a combination of crash type and severity, behavioral components, and facility characteristics. Countermeasures were selected from the FHWA’s List of Proven Safety Countermeasures based on the degree to which they could be expected to reduce crashes resulting from the identified risk factors and other considerations.

A periodic review of crash data and ongoing research of additional or alternative potential countermeasures will help to support the selection and implementation of the most appropriate and cost-effective safety measures in the City of Burlington.

APPENDIX B – EQUITY ANALYSIS

Equity Analysis and Data Discussion:

Since the 2021 HEAL Act passed in Washington, state agencies have been directed to implement recommendations from the Environmental Justice Task Force. That task force has recommended the use of 4 demographic variables to ensure that transportation investments are balanced in a way that promotes better outcomes for overburdened populations. These variables include communities that are most marginalized and at risk:

- Communities of color
- Limited English proficiency
- Low-income communities
- Persons with disabilities

Data for these variables can be found on data.census.gov from the tables listed below.

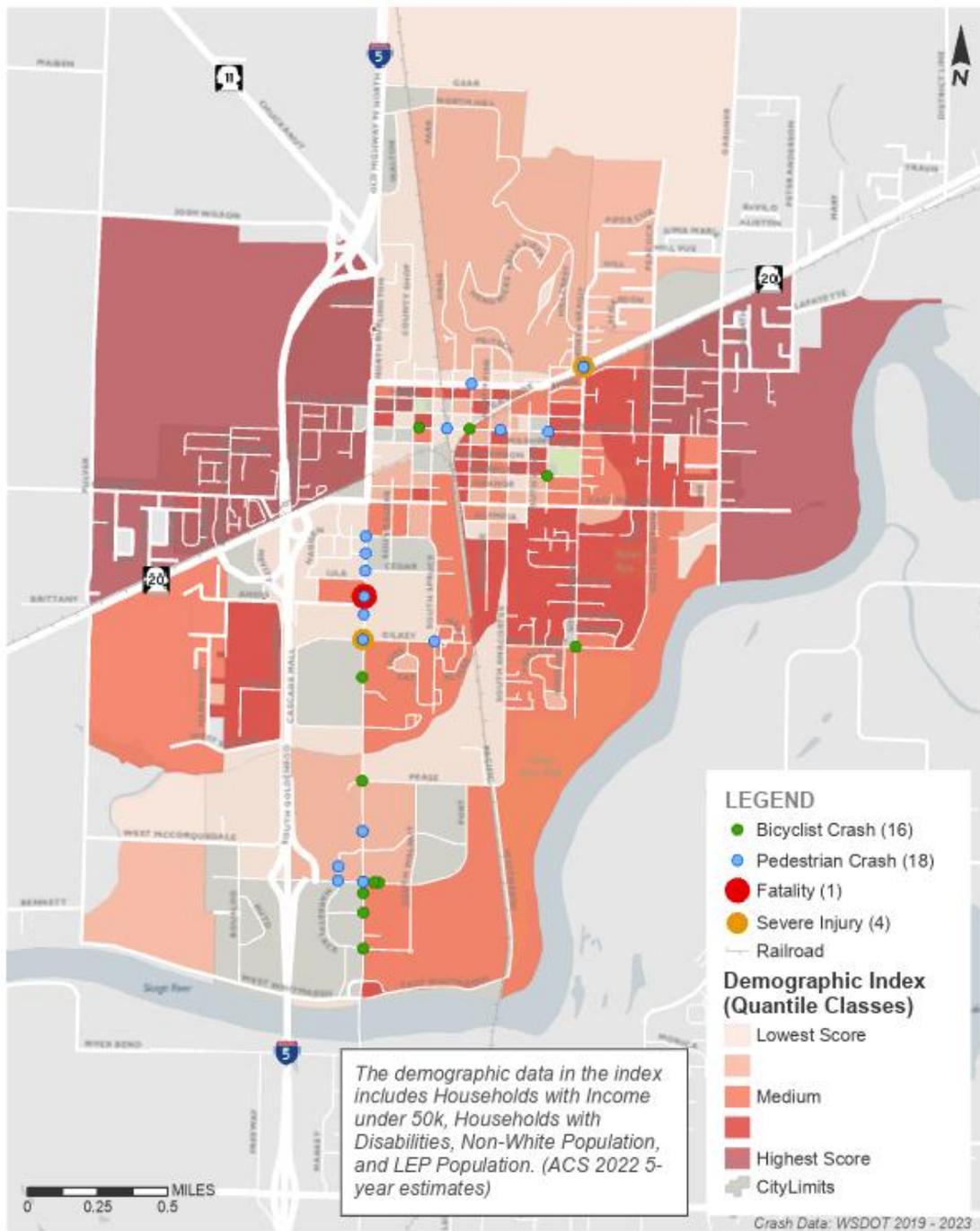
- B03002 (Hispanic or Latino Origin by Race)
- S1810 (Disability Characteristics)
- B16004 (Age by language spoken at home by ability to speak English for the population 5 years and over)
- S1701 Poverty Status in the past 12 months

These variable tables are published for **block groups** which is the second most detailed geography used by the census. However, for small cities, block groups are not detailed enough to understand where the communities of concern are located. A block group may cover several neighborhoods which planners want to consider separately. Census *blocks* are a useful enumeration unit in this case, but very few tables are available from the census at this level which makes it necessary to infer census block population data from the block groups by some other method.

Our preferred method for this is to use spatial analysis to infer census block populations, specifically the [enrich tool](#) from ESRI's business analytics toolset. This tool uses [data apportionment](#) to assign populations from the above variables to the smaller blocks by using the household and population statistics available for blocks.

GIS tool description:

Once the data from each of the population variables is calculated for blocks, we can create an **"Equity Index"** to help identify where the most vulnerable populations live. The Equity Index scores each block relative to all the blocks in the dataset by assigning it a value between 0 and 1 based on how close the value is to the minimum and maximum value for the dataset. For example, if there are 5 blocks, and each respective block has 1, 2, 3, 4, and 5 people living with a disability, then the block with 3 people living with a disability would get a score of 0.5 because the value is halfway between the minimum and maximum for all 5 blocks. Once each variable has been assessed this way, the min-max scores for each variable are then added up. This allows us to see which blocks have 'the most' of all the populations while still giving equal weight to each.



Pedestrian and Bicyclist Crashes and Demographic Index FIGURE XX
 Burlington Non Motorized and Safety Plan transpogroup

Figure B.1 Example demographic index score overlaid with bicycle & pedestrian crash data.
 Note: Exact scores not displayed since the value is in comparing blocks to each other.

Using this data for planning:

The outcome of the planning process will usually determine where and what type of infrastructure projects are needed. We can apply the equity index to these projects to determine which should have a higher priority for implementation. To do this, we create a spatial dataset of projects and buffer each project by 100 feet, as shown below:

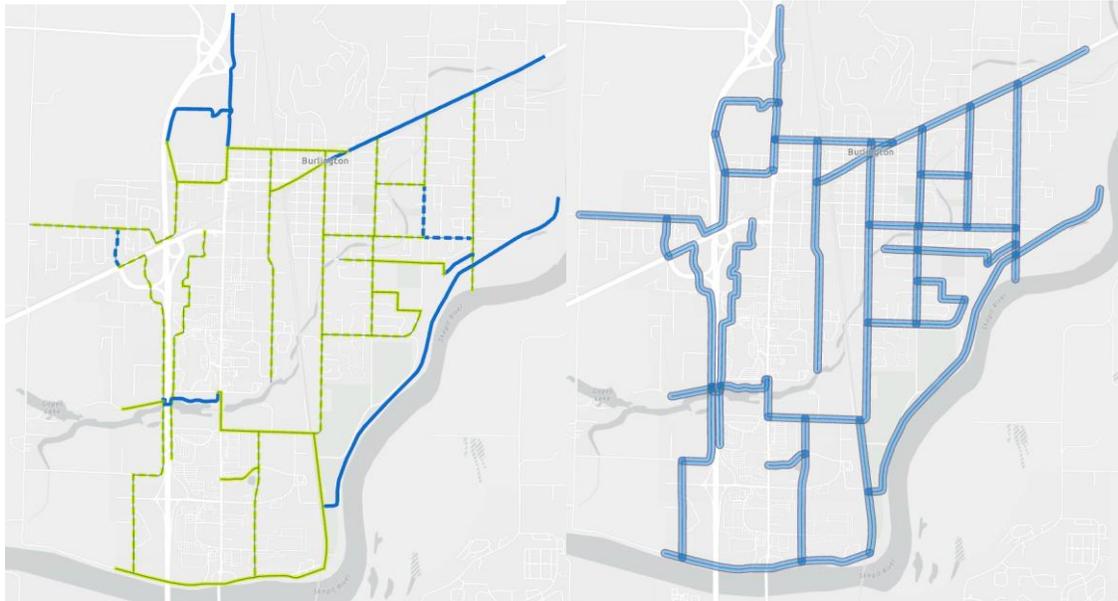


Figure B.2 – 100-foot buffer around identified projects

We then use the buffers to add up the index score of each of the blocks selected by the buffer:

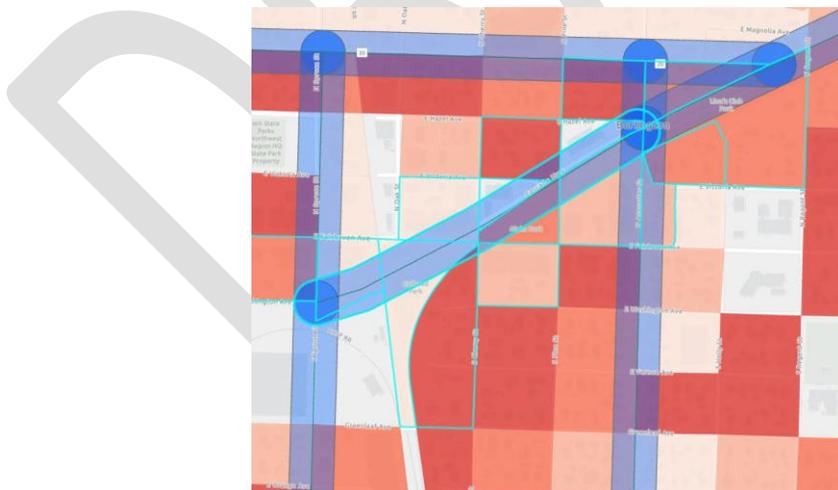


Figure B.3 – Buffers and blocks

The Equity Index score from each of the blocks selected by its outline is added to the buffer/projects total score. We can also choose to take the mean, or the max of all the blocks selected. The resulting map below shows where proposed bicycle improvements should have higher or lower priority based on the demographic characteristics of the populations living nearby.

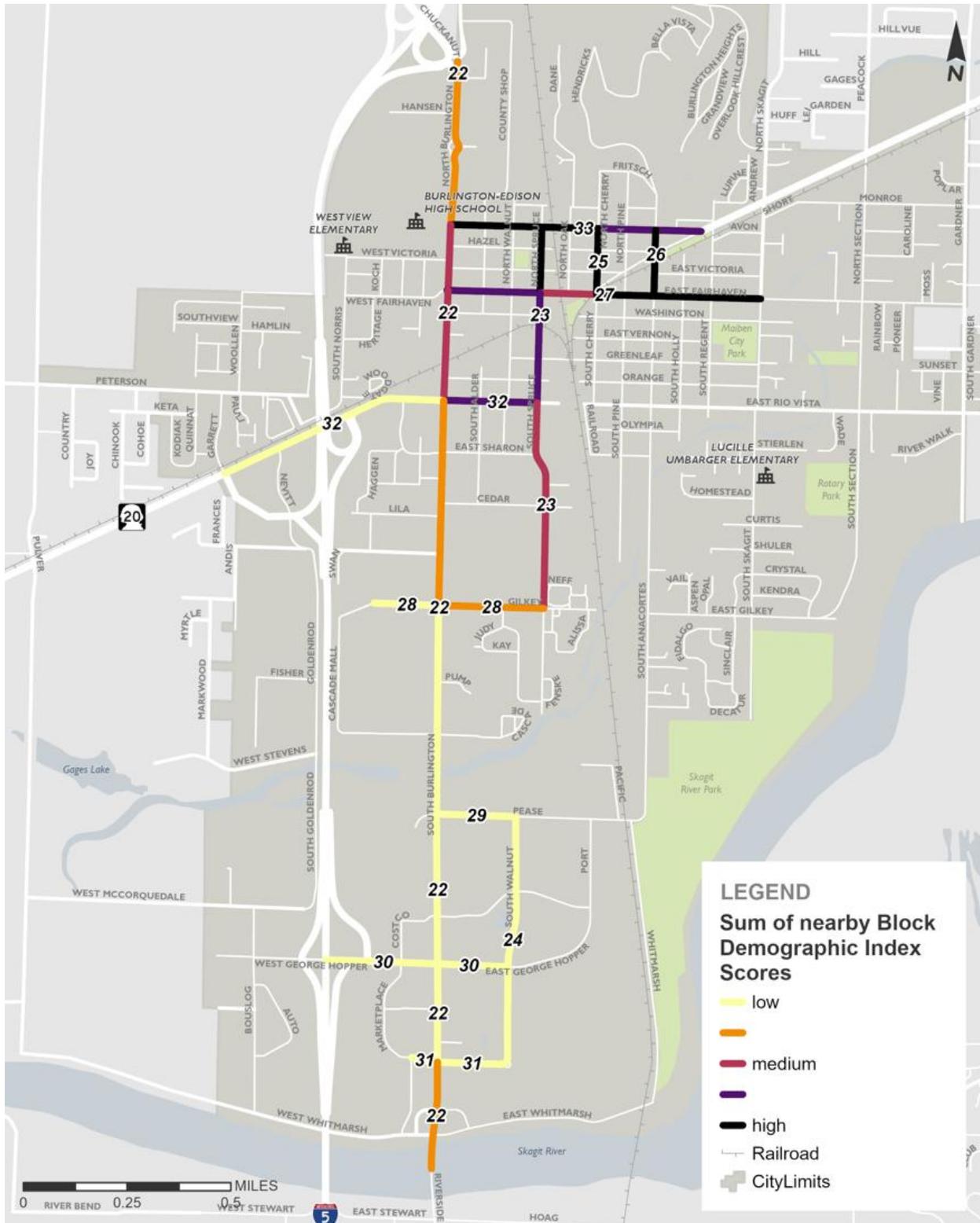


Figure B.4 – Example scoring summary of preliminary pedestrian network segments

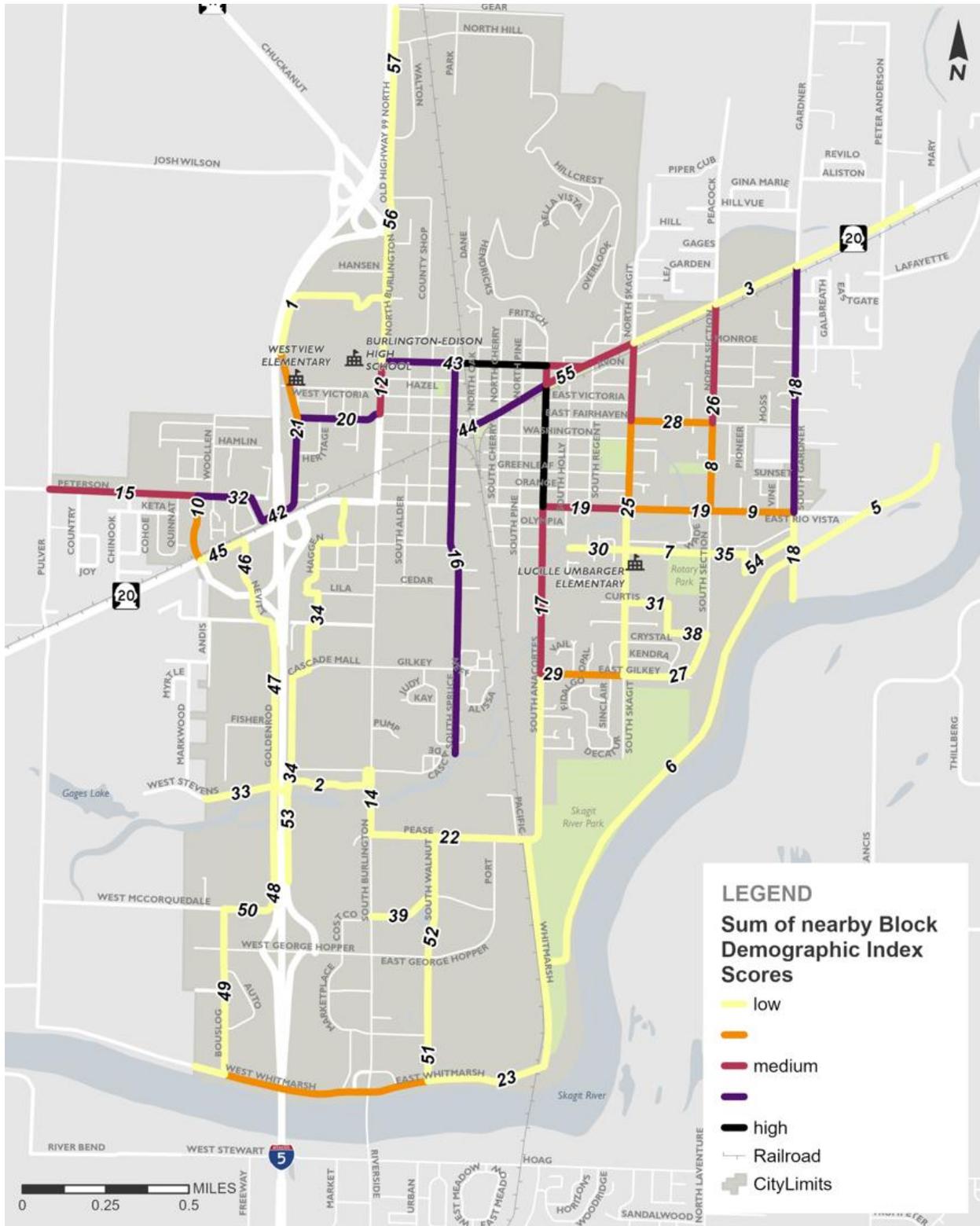


Figure B.5 – Example scoring summary of preliminary bicycle network segments

APPENDIX C – VEHICLE LOS ANALYSIS

Multimodal Level of Service Standards

A Level of Service (LOS) Standard is a way of describing a community’s minimum expectations for the performance of public services and facilities. The GMA requires that the Transportation Element of the Comprehensive Plan contain regionally coordinated Multimodal LOS (MMLOS) standards for all arterial streets, transit routes, and active transportation networks to serve as a gauge to judge performance of the system. MMLOS is both a quantitative and a qualitative measure used to assess various aspects of transportation system performance and completeness.

Multi-Modal Level of Service (MMLOS) Standards have been developed for Burlington’s transportation system, including:

- Active Transportation LOS standards that measure the completeness of pedestrian and bicycle networks (Policy 3.6)
- Transit LOS standards that assess ADA-compliance at Skagit Transit bus stop locations (Policy 3.7)
- **Vehicle LOS standards** to address traffic congestion on roadways and at intersections (LOS standard D for intersections and Burlington arterial streets)

Vehicle LOS Standards

Vehicular LOS is commonly used in transportation planning and engineering to evaluate how well a road or transportation facility is operating in terms of traffic flow, design capacity, and congestion. Vehicle LOS can be measured in various ways, but the most typical is Highway Capacity Manual (HCM) methodology for roadway capacity and intersection delay. Regardless of the method, standard transportation industry practice is to measure roadways or intersections at peak traffic demand (evening rush hour) periods.

Roadway Capacity LOS is simply a measure of actual or forecast traffic volume compared to a theoretical capacity for how many vehicles the roadway is designed to carry per hour. This is expressed as a volume to capacity ratio and assessed on a categorical scale from “A” to “F” (Table C.1.), with “A” representing free-flow traffic conditions and “F” indicating heavy traffic congestion.

Vehicular LOS Designation	V/C Range
A	0-0.59
B	0.60-0.69
C	0.70-0.79
D	0.80-0.89
E	0.90-0.99
F	>1.00

Signalized intersection LOS is defined in terms of a weighted average control delay for the entire intersection. Control delays quantify the increase in travel time that a vehicle experiences due to the traffic signal control as well as provides a surrogate measure for driver discomfort and fuel consumption. Signalized intersection LOS is stated in terms of average control delay per vehicle (in seconds) during a specified time period (e.g., weekday PM peak hour). Table C.2. summarizes the LOS criteria for signalized intersections, as described in the *Highway Capacity Manual 7th Edition* (Transportation Research Board, 2022).

Table C.2. Level of Service Criteria for Signalized Intersections

Level of Service	Average Control Delay (seconds/vehicle)	General Description
A	≤10	Free Flow
B	>10 – 20	Stable Flow (slight delays)
C	>20 – 35	Stable flow (acceptable delays)
D	>35 – 55	Approaching unstable flow (tolerable delay, occasionally wait through more than one signal cycle before proceeding)
E	>55 – 80	Unstable flow (intolerable delay)
F ¹	>80	Forced flow (congested and queues fail to clear)

Source: *Highway Capacity Manual 7th Edition*, Transportation Research Board, 2022, respectively.

1. If the volume-to-capacity (v/c) ratio for a lane group exceeds 1.0 LOS F is assigned to the individual lane group. LOS for overall approach or intersection is determined solely by the control delay.

Unsignalized intersection LOS criteria can be further reduced into two intersection types: all-way stop and two-way stop controlled. All-way stop controlled intersection LOS is expressed in terms of the weighted average control delay of the overall intersection or by approach. Two-way stop-controlled intersection LOS is defined in terms of the average control delay for each minor-street movement (or shared movement) as well as major-street left-turns. This approach is because major street through vehicles are assumed to experience zero delay, a weighted average of all movements results in very low overall average delay, and this calculated low delay could mask deficiencies of minor movements. Table C.3. shows LOS criteria for unsignalized intersections.

Table C.3. Level of Service Criteria for Unsignalized Intersections

Level of Service	Average Control Delay (seconds/vehicle)
A	0 – 10
B	>10 – 15
C	>15 – 25
D	>25 – 35
E	>35 – 50
F ¹	>50

Source: *Highway Capacity Manual 7th Edition*, Transportation Research Board, 2022.

1. If the volume-to-capacity (v/c) ratio exceeds 1.0, LOS F is assigned an individual lane group for all unsignalized intersections, or minor street approach at two-way stop-controlled intersections. Overall intersection LOS is determined solely by control delay.

Vehicle LOS Limitations

While traditional vehicle LOS measures provide important status information about roadway capacity and intersection operations during weekday peak demand times, they are incomplete and time-limited measures of a multimodal transportation system that do not provide a complete assessment or understanding of needs for improvement. As illustrated in Figure C.1 below, while there may be traffic congestion on a road or at an intersection at the busiest time of day, with demand exceeding capacity, during most other times of day there is excess and unused capacity.

Vehicle LOS measures:

- Count vehicles, but not how many people are in the vehicles (occupancy).
- Determine vehicle throughput (capacity) and driver inconvenience (seconds of delay).
- Imply that vehicle counts (demand) exceeding roadway or intersection capacity (supply) equates to “failure” of the facility without recognition that congestion is a temporary and inconvenient, but normal condition in an urban environment at the busiest time of day.
- Exceeding LOS threshold requires physical mitigation to add roadway capacity (lanes) or intersection capacity (turn lanes/storage/signal time)
- Have traditionally led to widening roadways and intersections, which has compromised needs for people walking, biking, rolling, and riding transit.

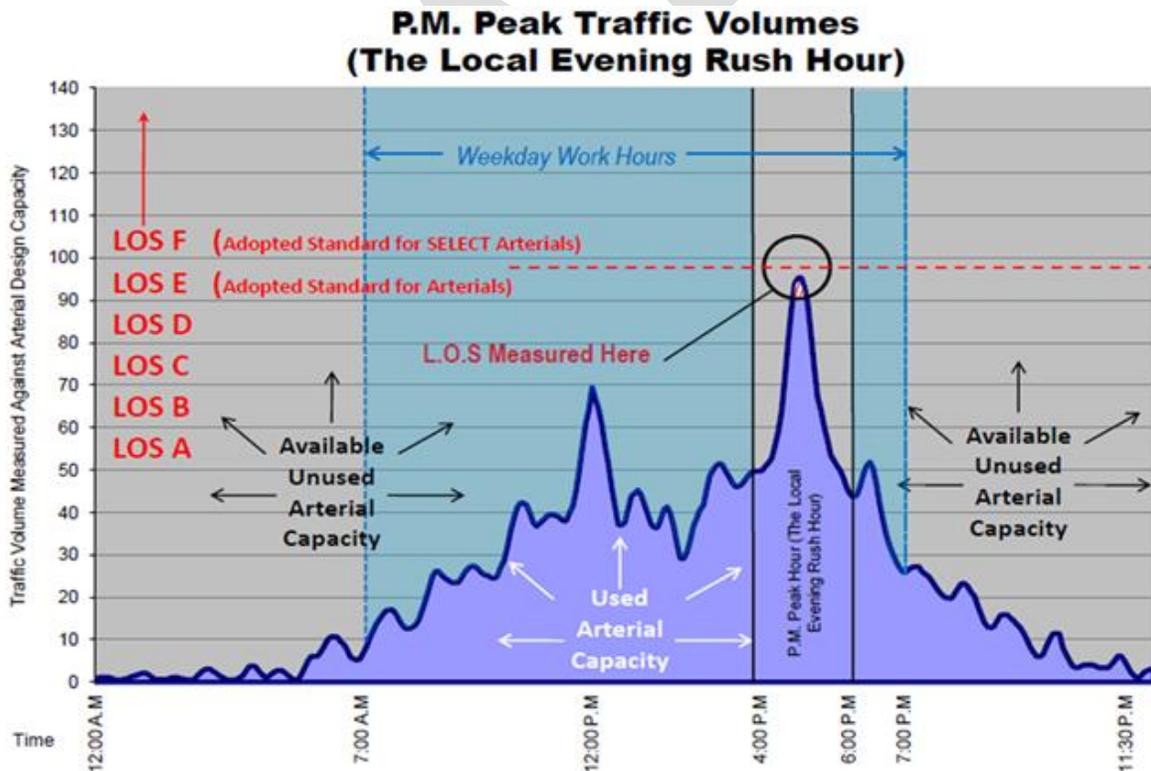


Figure C.1 Illustration of a Typical Arterial Street Traffic Volume at PM Peak Hour.

City Streets and Intersections

The City of Burlington has adopted vehicle LOS standards, as follows:

- LOS standard D for intersection delay
- LOS standard D for all other City arterial street capacity

State Highways

Interstate 5 (I-5) and State Route (SR) 20 are designated as Highways of Statewide Significance (HSS) with vehicle LOS standards set by WSDOT while SR 11 is a Highway of Regional Significance (HRS) with a vehicle LOS standard set by both WSDOT and SCOG.

I-5 runs north-south from Canada through Washington, Oregon, and California to Mexico.

- **LOS standard D** in the Urbanized Area and **LOS C** for the rest of the Skagit region.

SR 20 runs north-south from Deception Pass in Island County to Sharpe's Corner and then generally east-west through Burlington and Skagit County before turning briefly northward and into Whatcom County, eventually resuming a southeast course and terminating in the northeastern part of the state at Newport on the Washington-Idaho border. **SR 20 Spur** connects Sharpe's Corner through the City of Anacortes to the WSDOT terminal for vehicle and passenger ferry service to the San Juan Islands.

- **LOS standard D** in the Burlington Urban Area and **LOS C** for the rest of the Skagit region.

SR 11 (aka Chuckanut Drive) runs north-south from I-5 in Burlington through western Skagit County and along the coast through the Chuckanut Mountains to Bellingham.

- **LOS standard C** throughout the rural Skagit region.

Existing Intersection LOS

In 2024, the City of Burlington commissioned turning movement counts at several intersections and LOS calculations were made to establish a baseline for existing conditions. Table C. 4 and Figure C.2 show that there is one City intersection (Pease/Whitmarsh) and two SR 20 intersections (N Skagit and N Section) that currently operate below the City and WSDOT adopted LOS standard D threshold at PM peak hour. All three intersections are at BNSF railroad crossings and experience regular temporary blockage as trains pass, which contributes to increased delay and vehicle traffic congestion.

Count ID	Intersection Location		Agency	Traffic Control	Adopted LOS	Existing and Forecast Intersection LOS (HCM)					
	East/West Road	North/South Road				2024	Delay	WM	2045	Delay	WM
1	George Hopper Rd - Chrysler Dr	Bouslog Rd	City	AWSC	D	B	10.9	-	B	12.2	-
2	E Rio Vista Ave	Spruce St	City	TWSC	D	D	34.7	EB	F	163.0	EB
3	E Rio Vista Ave	S Anacortes St	City	TWSC	D	B	12.5	WB	C	16.3	EB
4	Greenleaf Ave	S Spruce St	City	TWSC	D	C	20.8	WB	E	36.6	WB
5	Pease Rd - S Anacortes St	E Whitmarsh Rd	City	TWSC	D	E	47.2	NBL	F	277.5	NBL
6	E Fairhaven Ave	S Skagit St	City	AWSC	D	B	10.0	-	B	14.0	-
7	Pease Rd - Driveway	S Burlington Blvd	City	Signal	D	B	16.6	-	C	21.1	-
8	George Hopper Rd - Chrysler Dr	S Burlington Blvd	City	Signal	D	C	25.2	-	C	30.0	-
9	Avon Ave (SR 20)	N Burlington Blvd (SR 20)	WSDOT	Signal	D	B	17.0	-	C	25.7	-
10	Avon Ave (SR 20)	N Spruce St	WSDOT	TWSC	D	D	25.5	SB	F	301.3	NB
11	Avon Ave (SR 20)	N Cherry St	WSDOT	TWSC	D	D	30.1	NB	F	67.9	NB
12	Cascade Hwy (SR 20)	N Regent St	WSDOT	TWSC	D	C	27.9	SB	F	75.5	SB
13	Cascade Hwy (SR 20)	N Skagit St	WSDOT	TWSC	D	F	100.9	SBTL	F	838.7	SBTL
14	Cascade Hwy (SR 20)	N Section St	WSDOT	TWSC	D	F	126.9	SBTL	F	518.8	NB

Acronym Notes
 HCM = Highway Capacity Manual; Delay = Average Seconds of Delay per Vehicle
 AWSC = All Way Stop Control; TWSC = Two-Way Stop Control; Signal = Signalized Intersection; RAB = Roundabout
 WM = Worst Movement; L = Left Turn; R = Right Turn; S = Straight Thru; EB = Eastbound; WB = Westbound; NB = Northbound; SB = Southbound; TL = Turn Lane

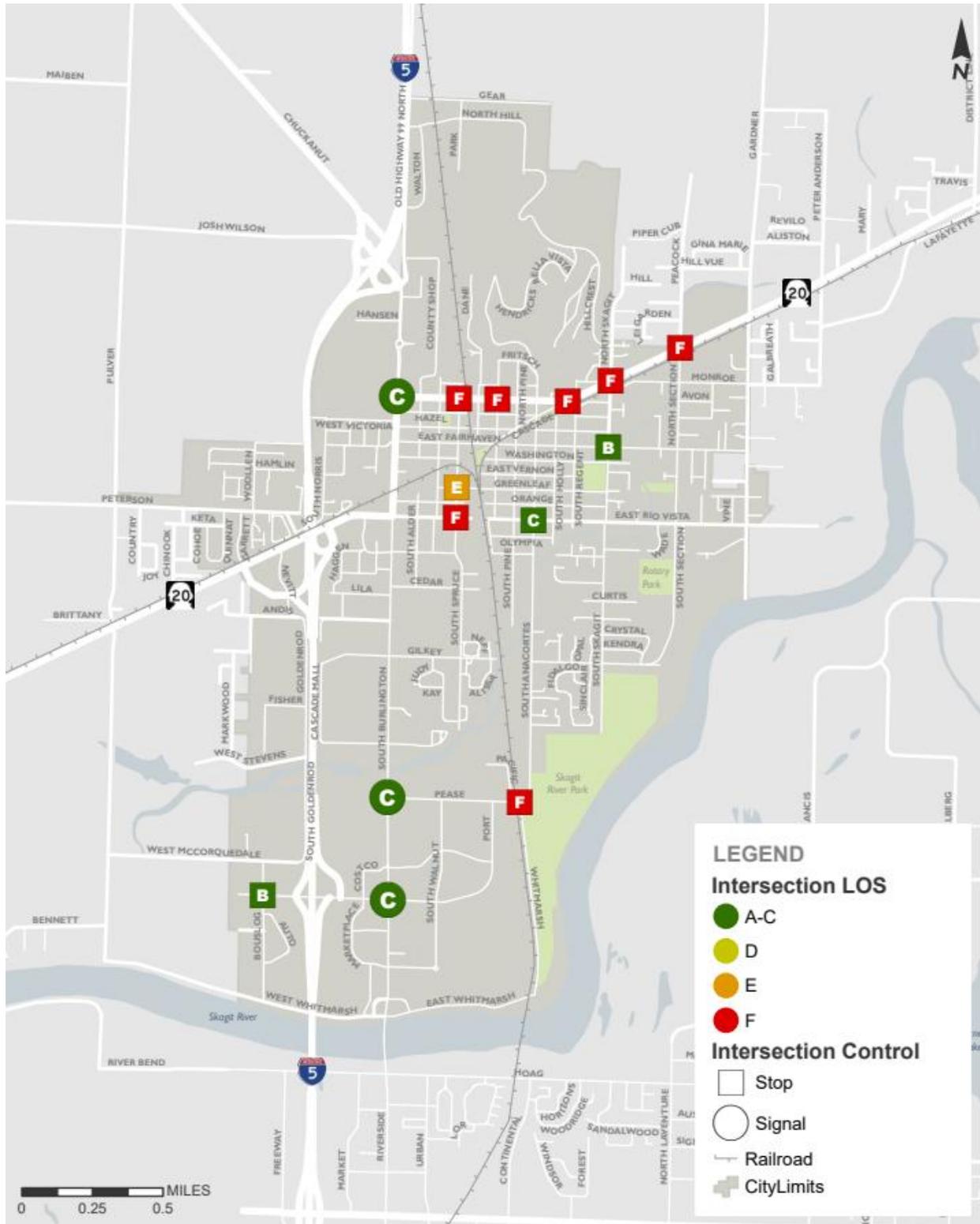


Figure C.3 2045 Forecast of Intersection LOS Status in Burlington, WA

2045 Future Forecast

Figure C.3 and Table C.5 show that in 2045, based on 2024 intersection turning movement counts, SCOG travel demand model growth rates, and Highway Capacity Manual methodology, eight intersections are forecast to exceed adopted City and WSDOT LOS standard D, with one forecast to operate at LOS standard E and seven forecast to operate at LOS standard F during the PM peak hour. The City will continue to monitor these intersections over time and, if warranted, develop mitigation measures to maintain vehicle LOS on City streets.

Count ID	Intersection Location		Agency	Traffic Control	Adopted LOS	2045			Potential Improvements to Maintain LOS
	East/West Road	North/South Road				2045	Delay	WM	
1	George Hopper Rd - Chrysler Dr	Bouslog Rd	City	AWSC	D	B	12.2	-	
2	E Rio Vista Ave	Spruce St	City	TWSC	D	F	163.0	EB	Install 4-way stop or compact roundabout
3	E Rio Vista Ave	S Anacortes St	City	TWSC	D	C	16.3	EB	
4	Greenleaf Ave	S Spruce St	City	TWSC	D	E	36.6	WB	Install 4-way stop or compact roundabout
5	Pease Rd - S Anacortes St	E Whitmarsh Rd	City	TWSC	D	F	277.5	NBL	Traffic Signal and Railroad Safety Crossing
6	E Fairhaven Ave	S Skagit St	City	AWSC	D	B	14.0	-	
7	Pease Rd - Driveway	S Burlington Blvd	City	Signal	D	C	21.1	-	
8	George Hopper Rd - Chrysler Dr	S Burlington Blvd	City	Signal	D	C	30.0	-	
9	Avon Ave (SR 20)	N Burlington Blvd (SR 20)	WSDOT	Signal	D	C	25.7	-	
10	Avon Ave (SR 20)	N Spruce St	WSDOT	TWSC	D	F	301.3	NB	Uncertain; Access Mgmt; Turn Restrictions
11	Avon Ave (SR 20)	N Cherry St	WSDOT	TWSC	D	F	67.9	NB	Uncertain; Access Mgmt; Turn Restrictions
12	Cascade Hwy (SR 20)	N Regent St	WSDOT	TWSC	D	F	75.5	SB	Uncertain; Access Mgmt; Turn Restrictions
13	Cascade Hwy (SR 20)	N Skagit St	WSDOT	TWSC	D	F	838.7	SBTL	Uncertain; Access Mgmt; Turn Restrictions
14	Cascade Hwy (SR 20)	N Section St	WSDOT	TWSC	D	F	518.8	NB	Uncertain; Access Mgmt; Turn Restrictions

Acronym Notes

HCM = Highway Capacity Manual; Delay = Average Seconds of Delay per Vehicle

AWSC = All Way Stop Control; TWSC = Two-Way Stop Control; Signal = Signalized Intersection; RAB = Roundabout

WM = Worst Movement; L = Left Turn; R = Right Turn; S = Straight Thru; EB = Eastbound; WB = Westbound; NB = Northbound; SB = Southbound; TL = Turn Lane

For the three City intersections, potential mitigation measures could potentially include 4-way stop control, compact roundabouts, a traffic signal, and railroad safety crossing improvements.

For the five WSDOT intersections on SR 20, Intersection Control Evaluation (ICE) studies may need to be conducted to determine preferred or feasible intersection control options. While WSDOT has a 'roundabout first' policy, roundabouts may not be feasible at Avon Ave (SR 20)/ Spruce Street and Avon Ave (SR20)/Cherry Street due to right-of-way availability or the presence of buildings. The three intersections along SR 20 at Regent, Skagit, and Section Streets are very close to the BNSF railroad tracks and mitigation is uncertain.